

City of Langley Master Transportation Plan

Round 1 Engagement Summary Report – Draft
January 2021

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SECTION 1: INTRODUCTION

In September 2021, Langley City began the process of updating their existing Master Transportation Plan (MTP), building on the previous plan completed in 2014. This updated plan will help guide the City in creating a multimodal transportation network that provides safe and efficient movement of people and goods by all modes.

This Engagement Summary Report provides an overview of the key engagement activities utilized to seek input on the Transportation Master Plan, and summarizes the feedback received. Section 2 highlights the engagement process to date, with summaries of the Online Survey results provided in Section 3 and an overview of the feedback from the Online Public Consultation Meetings and Online Stakeholder Meetings in Section 4 and 5 respectively.

SECTION 2: ENGAGEMENT PROCESS

Throughout the development of the MTP, there will be two rounds of community engagement and a final survey to gather input on the draft MTP. The engagement process includes both public and stakeholder engagement, with the following activities completed in Round 1:

- An online survey hosted on ArcGIS StoryMaps and advertised on the City's website and social media channels.
- Two online public consultation meetings hosted via Zoom and advertised on the City's website and social media channels.
- One online stakeholder meeting hosted via Zoom. Attendees were invited based on identified core community stakeholder groups.

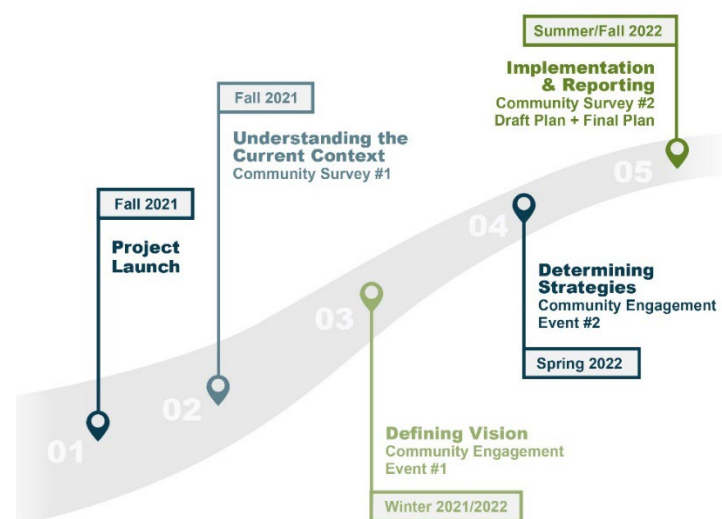


Figure 1: MTP Project Process and Phasing

SECTION 3: ONLINE SURVEY

An online ESRI StoryMap with an embedded mapping tool and a link to the SurveyMonkey survey was available for the public to complete between October 29th and November 30th, 2021. The survey and mapping exercise were designed to identify current transportation challenges and opportunities in Langley City, and better understand the community's priorities for future transportation improvements.

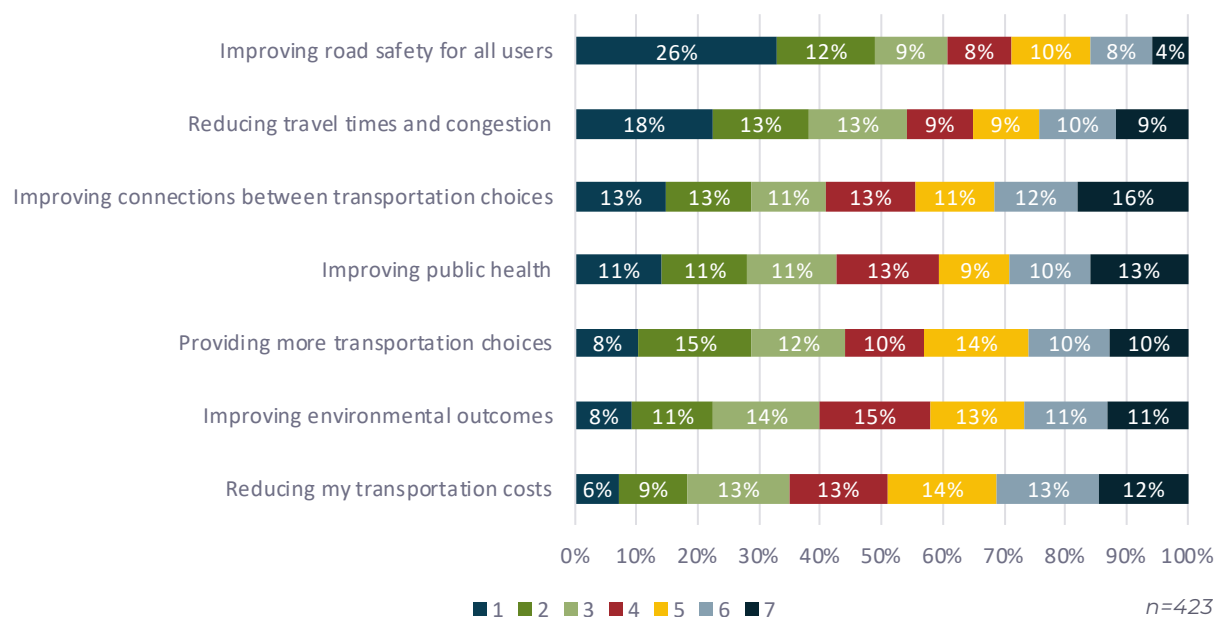
The survey was advertised on the City's website and social media pages, and received 426 responses, which are summarized below. The results of this survey will be used to inform the draft MTP, along with the results of the technical reviews.

Where applicable, the graphs represent responses by both the number of respondents and the percentage.

Identifying Priorities

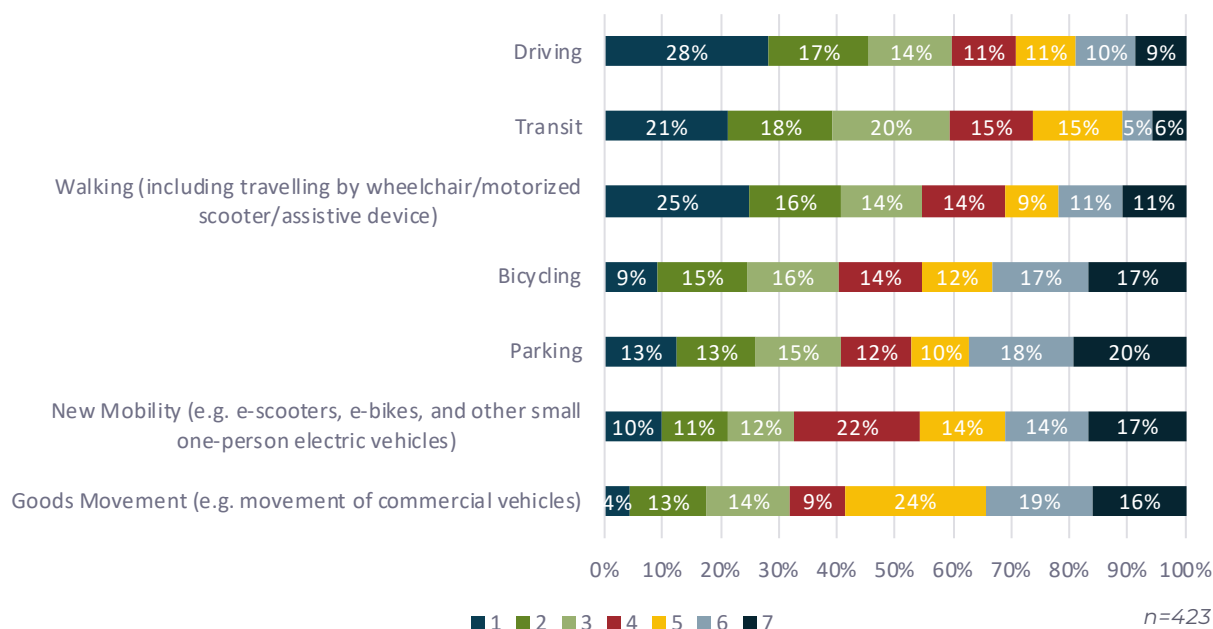
As the MTP is developed, which of the following outcomes are most important to you?

Survey respondents were asked to identify which outcomes were most important to them. A list of six outcomes were provided for participants to rank from 1 (most important) to 7 (least important). Improving road safety for all users was identified as the most important outcome among respondents, with an average ranking of 3.08, followed by reducing travel times and congestion which received an average ranking of 3.57. Reducing transportation costs was identified as the lowest priority among respondents with an average ranking of 4.34.



What aspects of Langley City's transportation system should be considered the highest priority?

Respondents were presented with a list of six topics and asked to rank their top priority areas for Langley City's transportation system from 1 (most important) to 7 (least important). Driving and transit were identified as the top priorities among respondents with identical average rankings of 3.23, followed by walking receiving an average ranking of 3.44. Goods movement was the lowest priority among respondents with an average ranking of 4.55.



What are the top three transportation issues or challenges facing Langley City today?

392 respondents answered this question with 1084 comments. These comments were reviewed in detail and key themes were identified, as shown below:

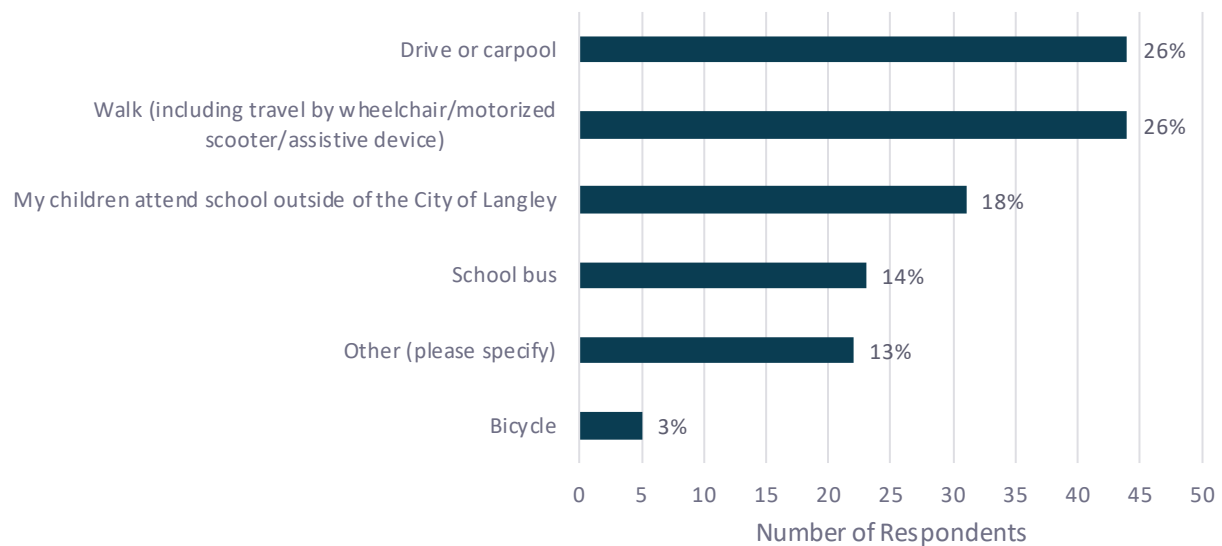
- Too much congestion and traffic issues (312)
- Public transit (142)
 - Lack of services and amenities
 - Excessive wait times
 - Want SkyTrain
- Pedestrians (including mobility assistive devices) (81)
 - Lack of infrastructure
 - Sidewalks and pathways are not well maintained
- Safety concerns (personal safety from environment/community factors and vehicles not obeying rules of the road) (68)
- Lack of parking (93)
- Bicycling (80)
 - Lack of infrastructure (bicycle lanes)
 - Too much focus on cycling infrastructure, taking away road space
- Roads and road maintenance (69)
 - Too few lanes
 - Roads are not well maintained

- Lack of connections around Langley City and to destinations outside of the City (35)
- Too much development and increased population (52)
- Too much commercial vehicle traffic (19)
- Too much railway traffic (18)
- Lack of alternatives to driving (18)

Travel Patterns

If you have school-aged children that attend school in Langley City, how do they typically travel to school?

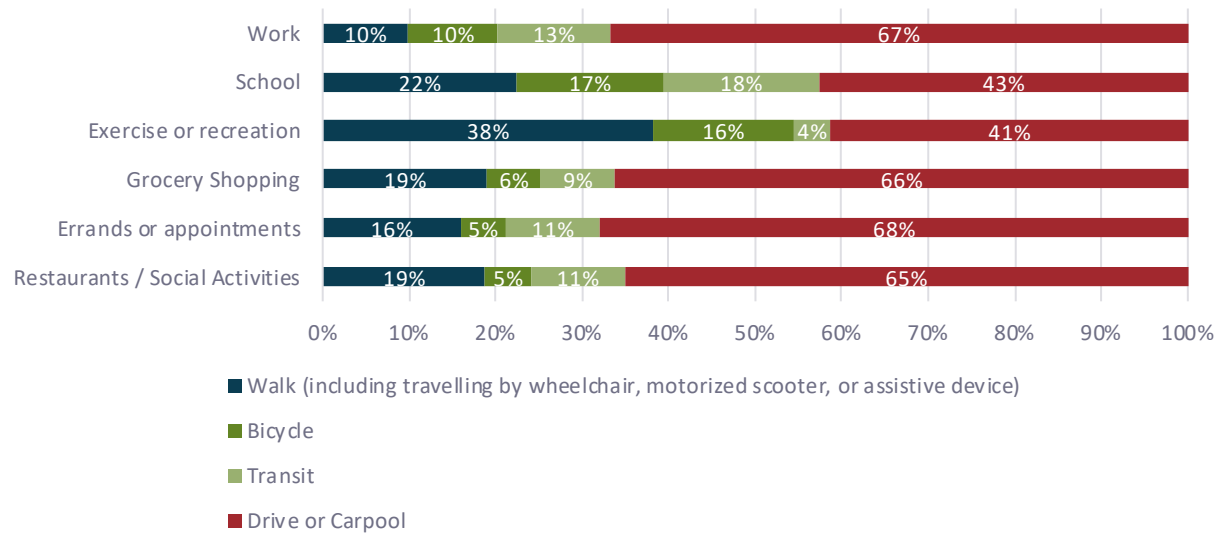
More than half of respondents identified that they do not have children/do not have children of school age. Of those who did identify having children of school age, an equal number of respondents said their children are driven/carpooled or walk to school.



n= 169 (238 respondents identified they do not have children/do not have school aged children)

How do you typically travel to/from the following?

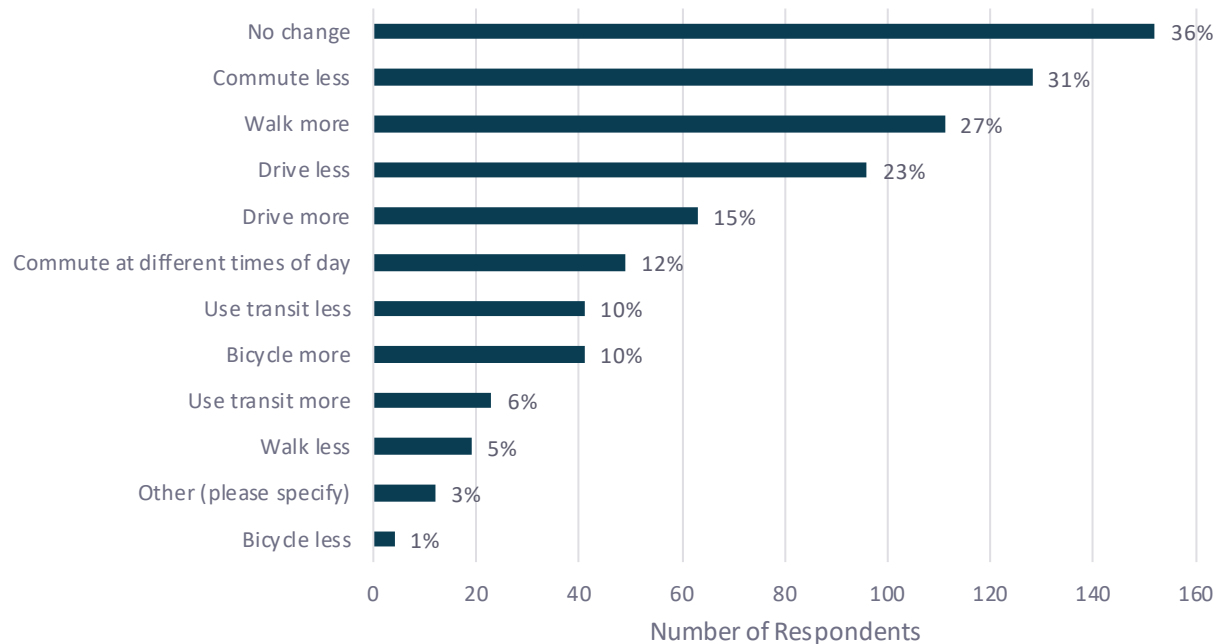
Survey respondents were asked how they typically travel to and from popular destinations. Driving or carpooling was the top mode of transportation for all destination options. Walking was the next most common response, but was less popular than driving or carpooling.



n = 418 (summary excluding N/A responses)

In general, how has COVID-19 impacted your transportation habits and travel patterns?

More than a third of respondents said that there has been no change in their travel habits due to Covid-19. Of those respondents whose travel patterns have changed, 31% said that they commute less. Those who selected other specified that they leave the house less (3) and have stopped using transit (3).



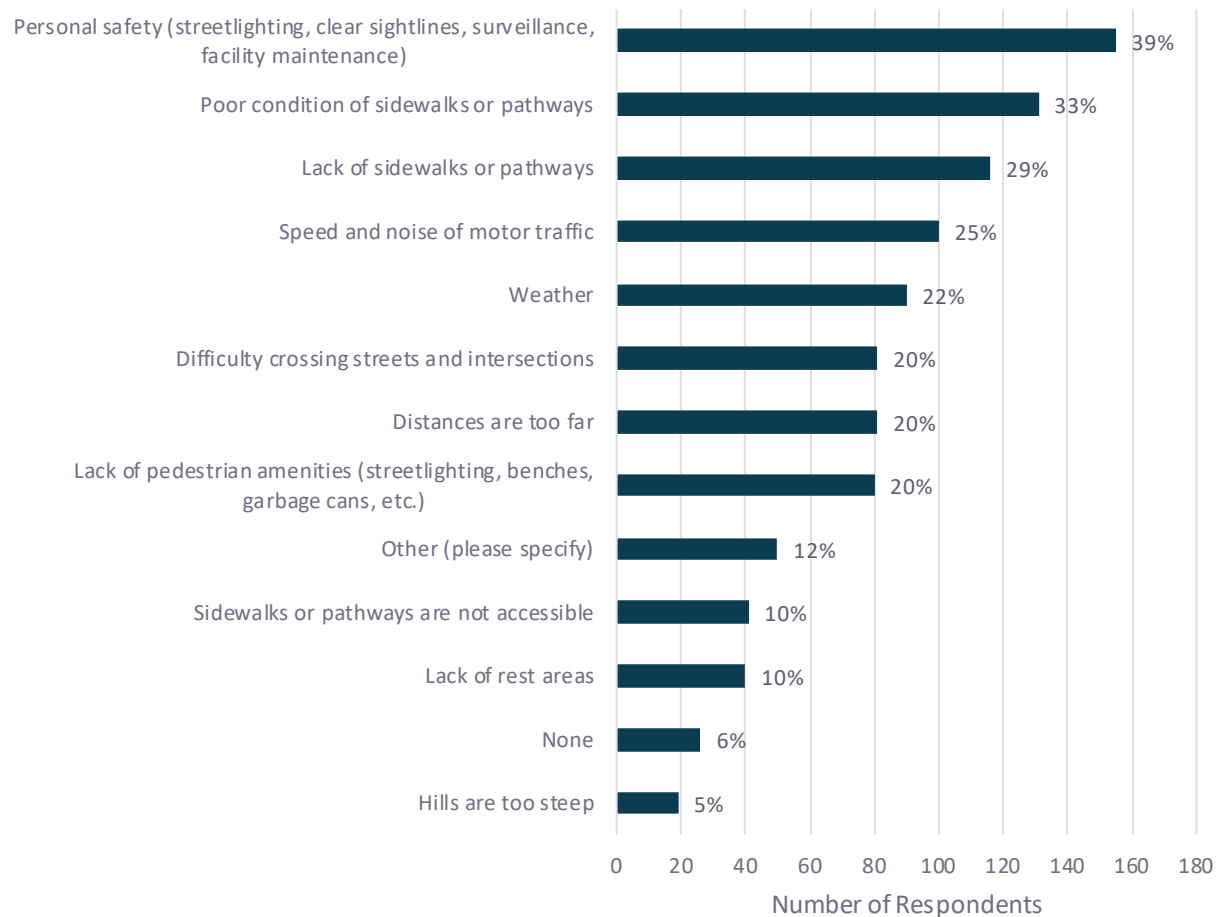
n = 417

Issues and Opportunities

Walking

What are the main issues or challenges for walking in the Langley City?

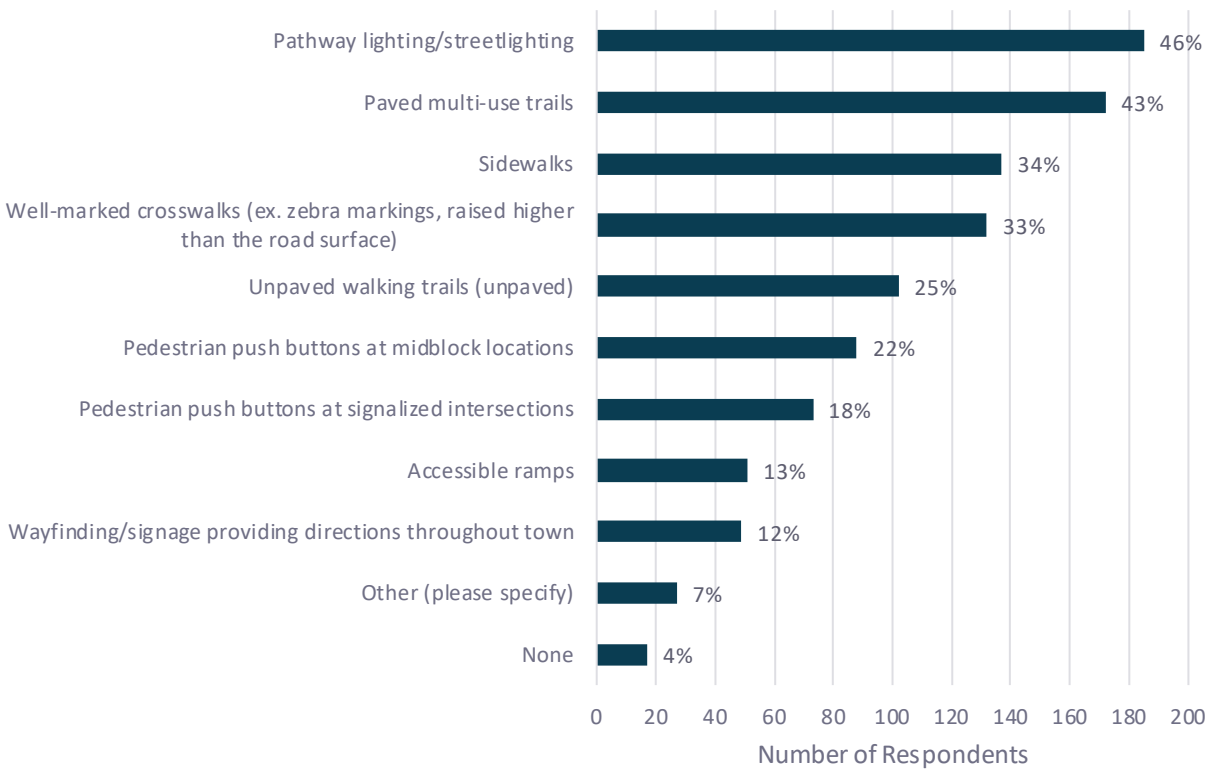
Respondents were asked to identify the top three issues or challenges for walking in the City of Langley. *Personal safety, poor condition of sidewalks or pathways, and lack of sidewalks or pathways* were identified as the top three issues or challenges. These responses were closely followed by *speed and noise of motor traffic*.



n = 402

What types of walking infrastructure would you like to see more of in Langley City?

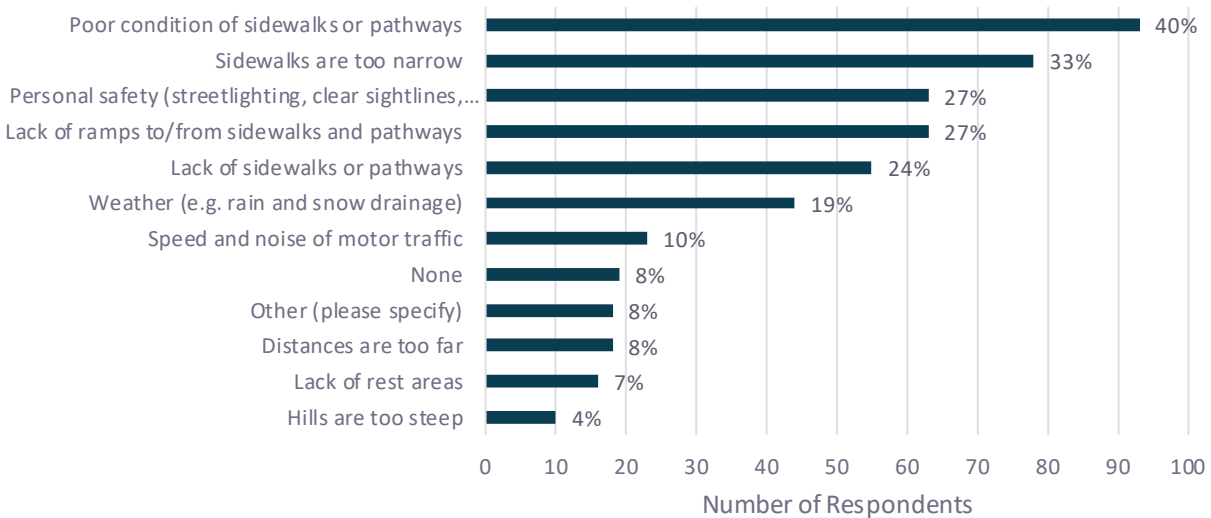
When asked what walking infrastructure respondents would like to see more of in the Langley City, *pathway lighting/streetlighting* (46%), *paved multi-use trails* (43%), and *sidewalks* (34%) were the most popular responses among respondents. *Other* included ideas such as more pedestrian signalized intersections (x5), additional pedestrian amenities including rest areas and washrooms (x3), and improving sidewalks (x3).



n = 402

What are the main issues or challenges for using a wheelchair, motorized scooter, or other assistive device in Langley City?

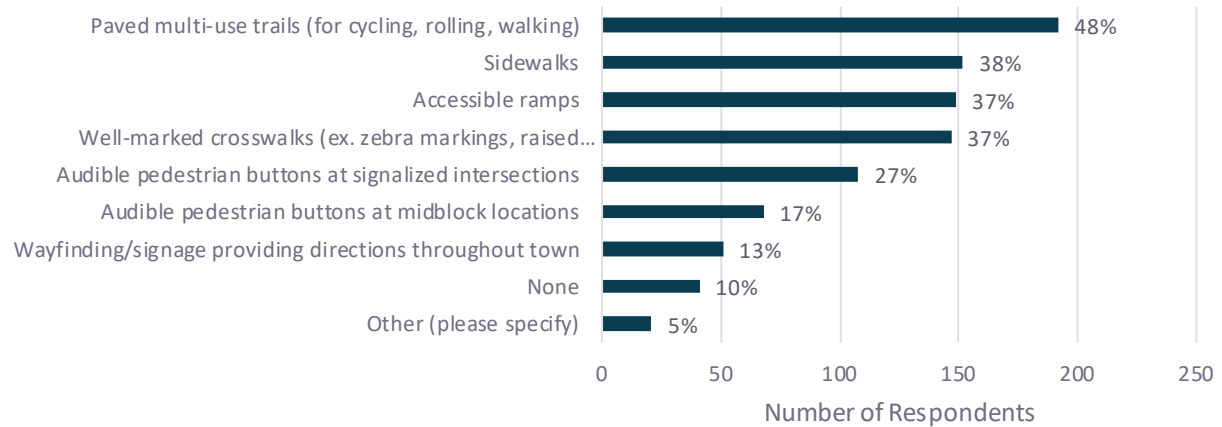
When asked to identify their top 3 issues or challenges for using a wheelchair, motorized scooter, or other assistive device in Langley City, 40% of respondents selected *poor condition of sidewalks or pathways*. This is followed by 33% of respondents who identified *sidewalks are too narrow*. *Personal safety* and *lack of ramps to/from sidewalks and pathways* were both selected by 27% of respondents.



n= 234 (summary excluding "I do not use a wheelchair, motorized scooter or other assistive device" responses)

What types of accessible infrastructure would you like to see more of in Langley City?

When asked what accessible infrastructure they would like to see more of in the Langley City, *paved multi-use trails* (48%), *sidewalks* (38%), *accessible ramps* (37%), and *well-marked crosswalks* (37%) were the most popular responses.

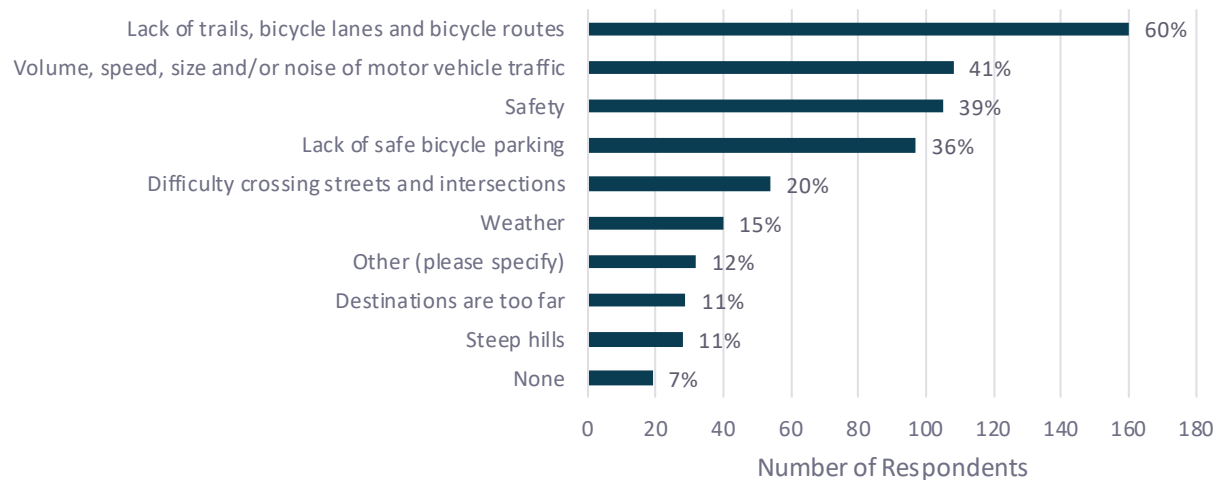


n = 402

Bicycling

What are the main issues or challenges for bicycling in the Langley City?

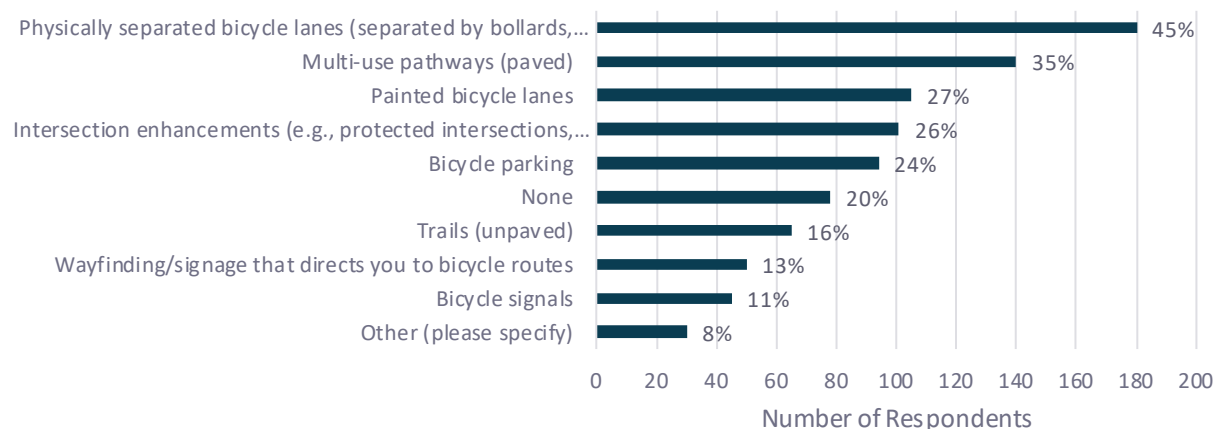
When asked to identify their top three issues or challenges for bicycling in Langley City, respondents identified the *lack of trails, bicycle lanes and bicycle routes* (60%), *volume, speed, size and/or noise of motor vehicle traffic* (41%), and *safety* (39%).



n = 266 (summary excluding "I do not bicycle" responses)

What types of bicycling infrastructure would you like to see in Langley City?

When asked what type of cycling infrastructure they would like to see, survey respondents indicated that they would like to see more *physically separated bicycle lanes* (45%), *multi-use pathways* (35%), and *painted bicycle lanes* (27%).

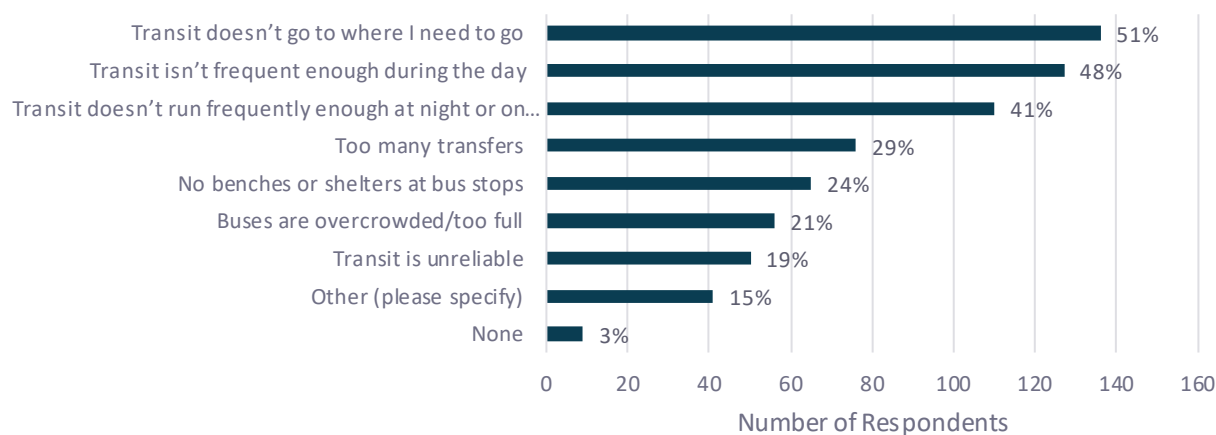


n = 396

Transit

What are the main issues or challenges for transit in Langley City?

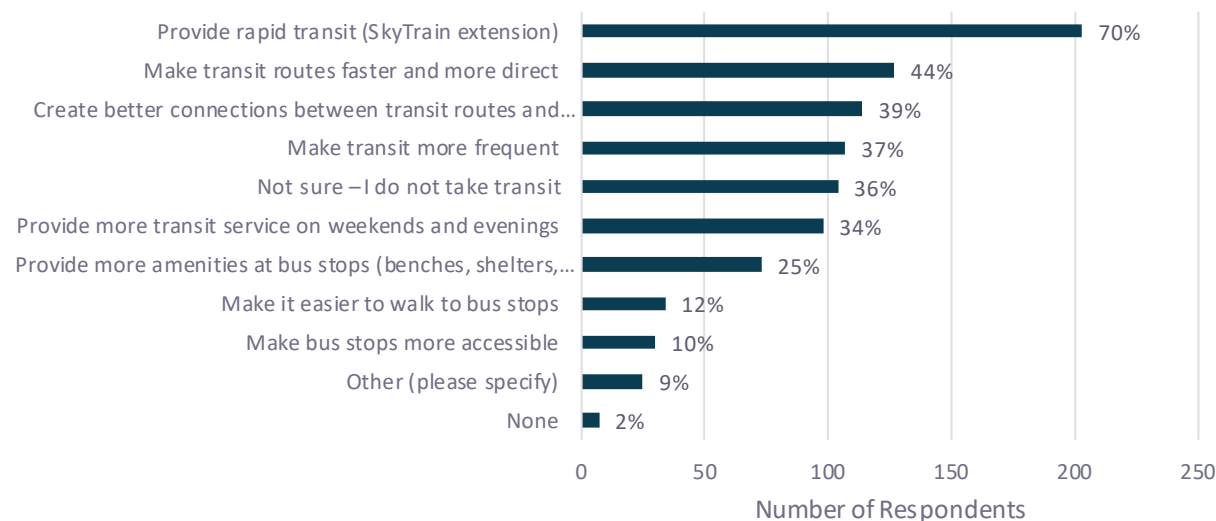
When asked to identify the main issues or challenges for transit in Langley City, respondents said that *transit doesn't go where I need to go* (51%), *transit isn't frequent enough during the day* (48%), and *transit doesn't run frequently enough at night or on weekends* (41%).



n = 266 (summary excluding "I do not take transit" responses)

What transit improvements would you like to see in Langley City?

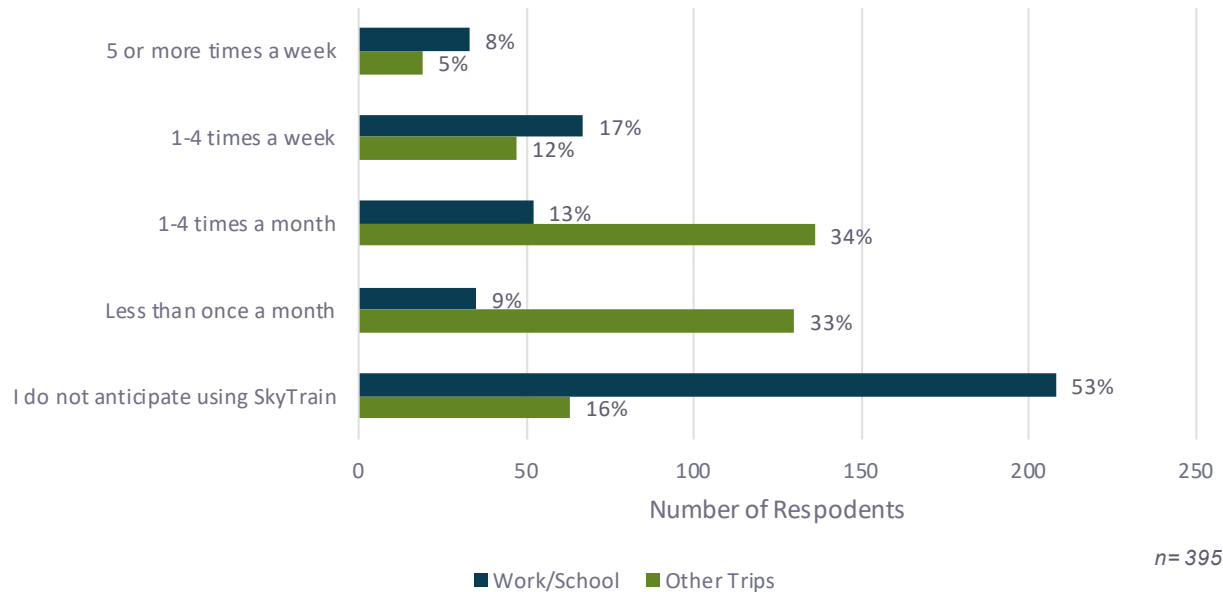
To improve the transit system, respondents said they would like to see the City *provide rapid transit (Skytrain extension)* (70%), *make transit routes faster and more direct* (44%), *create better connections between transit routes and options* (39%), and *make transit more frequent* (37%).



n = 291

How often do you anticipate using SkyTrain?

With the development of the new Surrey-Langley SkyTrain extension which has one station planned for Langley City, respondents were asked how frequently they anticipate using SkyTrain once the new extension is completed. 53% of respondents *do not anticipate using SkyTrain for work or school trips*. 25% of respondents said that they *anticipate using SkyTrain for work or school trips multiple times a week*. 34% of respondents said that they *anticipate using SkyTrain for other trips (attending events and social activities, shopping, and appointments) between 1-4 times a month*, and 33% anticipate using SkyTrain for those activities *less than once a month*.



Driving

What are the main issues or challenges for driving or carpooling in Langley City?

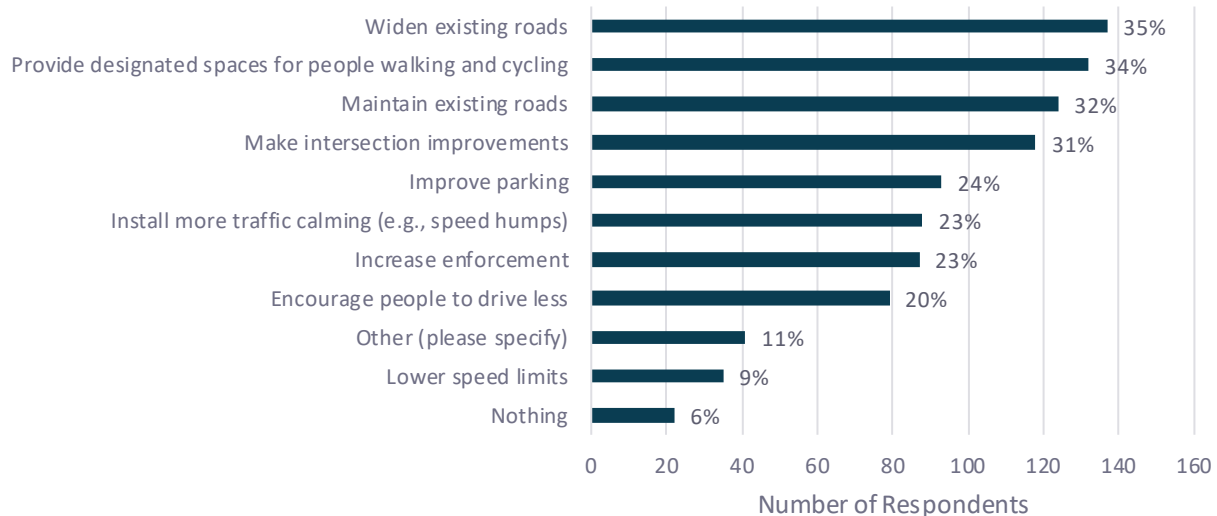
When asked to identify the main issues and challenges for driving or carpooling in Langley City, 75% of those who responded selected *too much congestion during rush hour*, followed by *too much congestion the rest of the day* (50%).



n= 368 (summary excluding "I don't have a car or drive" responses)

What could the City do to improve driving or carpooling in Langley City?

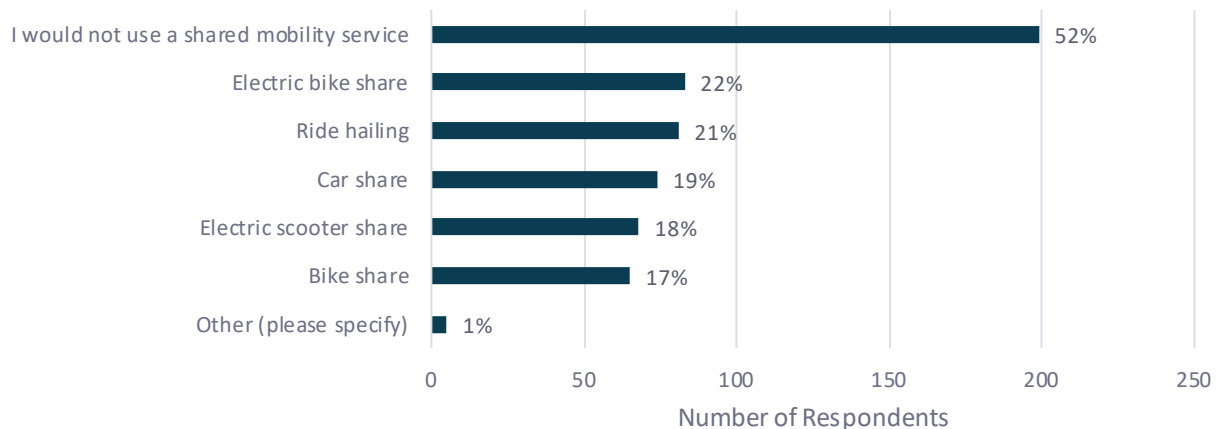
When asked what the City could do to improve driving or carpooling, *widen existing roads* (35%), *provide designated spaces for people walking and cycling* (34%), and *maintain existing roads* (32%) were the most popular responses among respondents.



n= 386

Would you use a shared mobility service in Langley City? If so, what kind would you use?

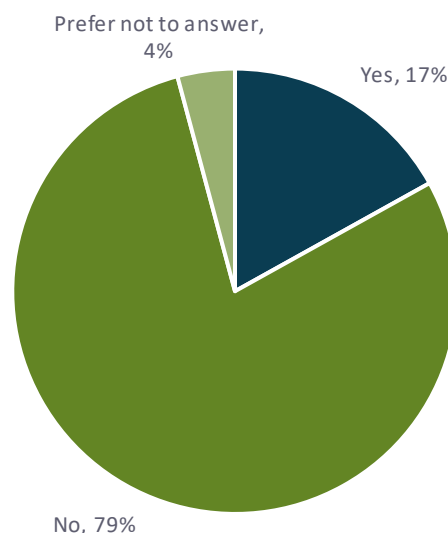
Respondents were asked if they would use a shared mobility service. 52% of respondents said they *would not use a shared mobility service*. For respondents who would use a shared mobility service, *electric bike share* (22%), *ride hailing* (21%), and *car share* (19%) were identified as the most desired.



n= 383

Have you faced any barriers or challenges related to transportation because of your race, national or ethnic origin, age, gender identity, sexual orientation, disability, family make up, conviction for an offense or other factor?

79% of respondents have not faced barriers or challenges related to transportation; however, 17% of respondents indicated that they have experienced barriers or challenges related to transportation as a result of their race, national or ethnic origin, age, gender identity, sexual orientation, disability, family make-up, conviction for an offence or other factor.



What are some examples of these transportation barriers or challenges?

51 respondents answered this question. Each response was reviewed in detail and key themes were identified, as detailed below.

Theme 1: Accessibility (25 comments)

Respondents noted accessibility challenges, such as accessing HandyDart, limited accessible parking options, sidewalks, and lack of bus stop amenities prevent them from being able to get around the city. Respondents also noted that affordability for transit and driving is a challenge.

Theme 2: Safety (18 comments)

Respondents noted that they do not feel safe due to people not obeying traffic rules, fear of interaction with people that are without homes, or hostile people on transit. Several respondents also said that they feel unsafe as a woman cyclist, pedestrian, and transit user.

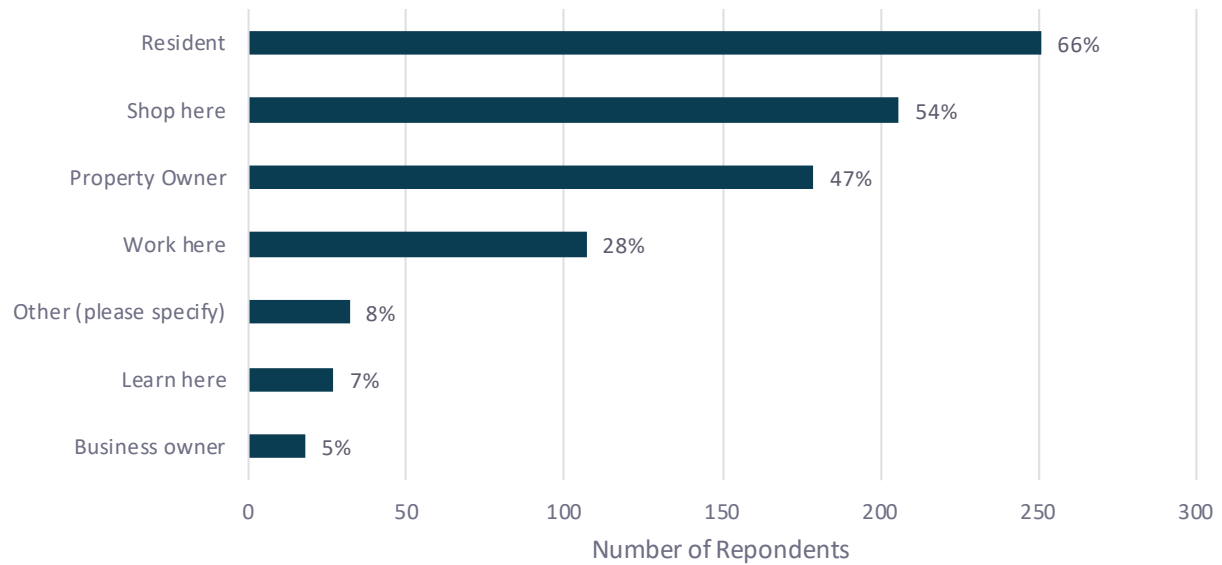
Theme 3: Time and convenience (8 comments)

Respondents noted that transit is not an efficient way to travel around the Langley City.

Demographics

What is your connection Langley City?

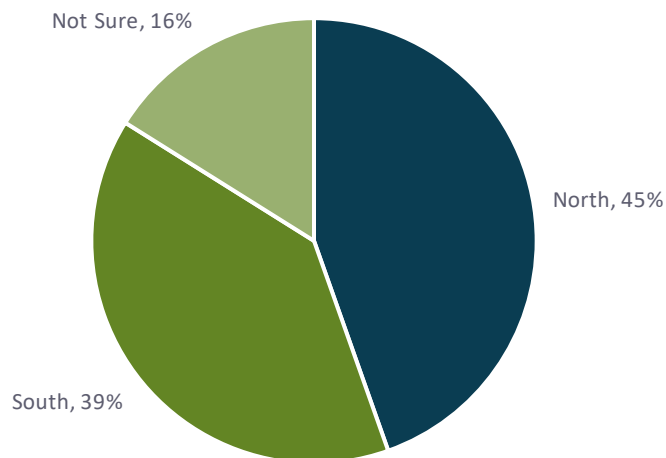
66% of respondents identify as residents of Langley City, and 54% said that they shop here.



n = 383

Do you live north or south of the Nicomekl River?

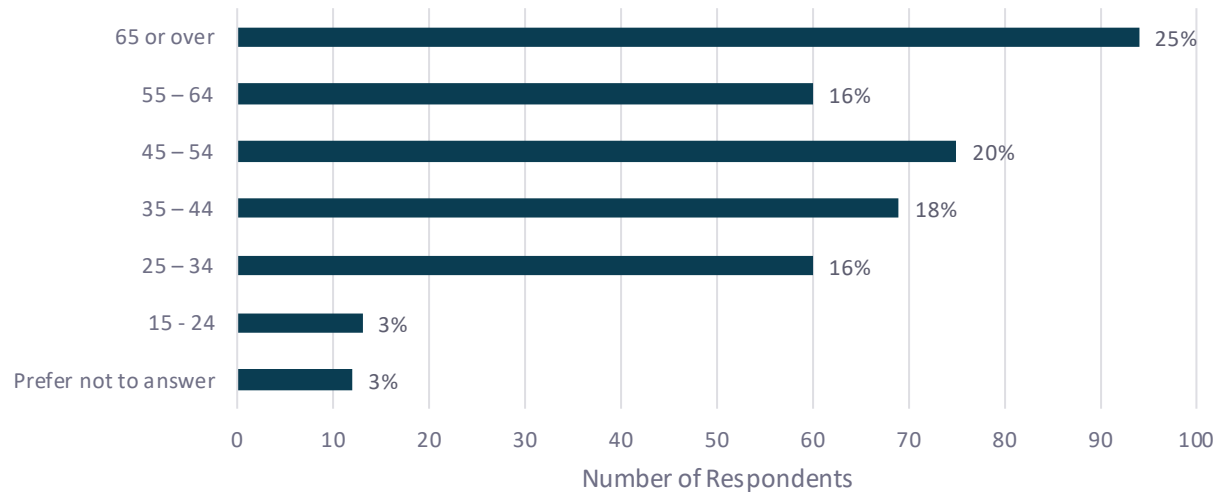
45% of respondents identified that they live north of the Nicomekl River, and 39% said they live south of the Nicomekl River. 16% of respondents were unsure.



n = 379

How old are you?

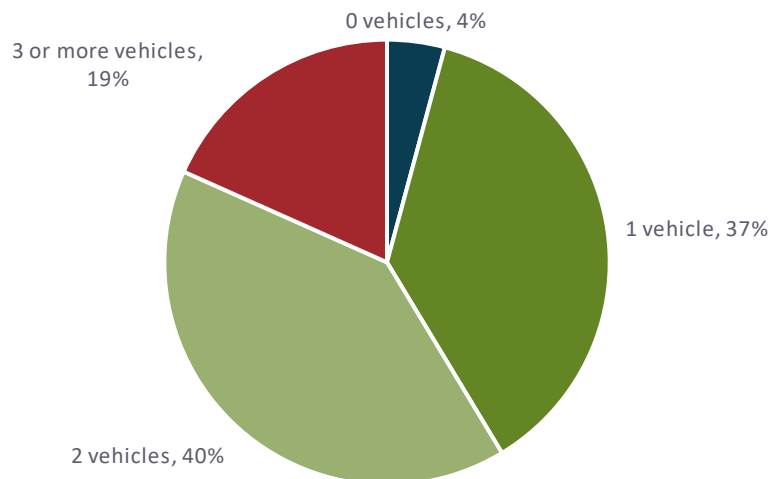
The majority of respondents (70%) are between the ages of 25 – 64, 25% are above the age of 64, and 3% are between the ages of 15 and 24.



n = 383

How many vehicles are registered to your household?

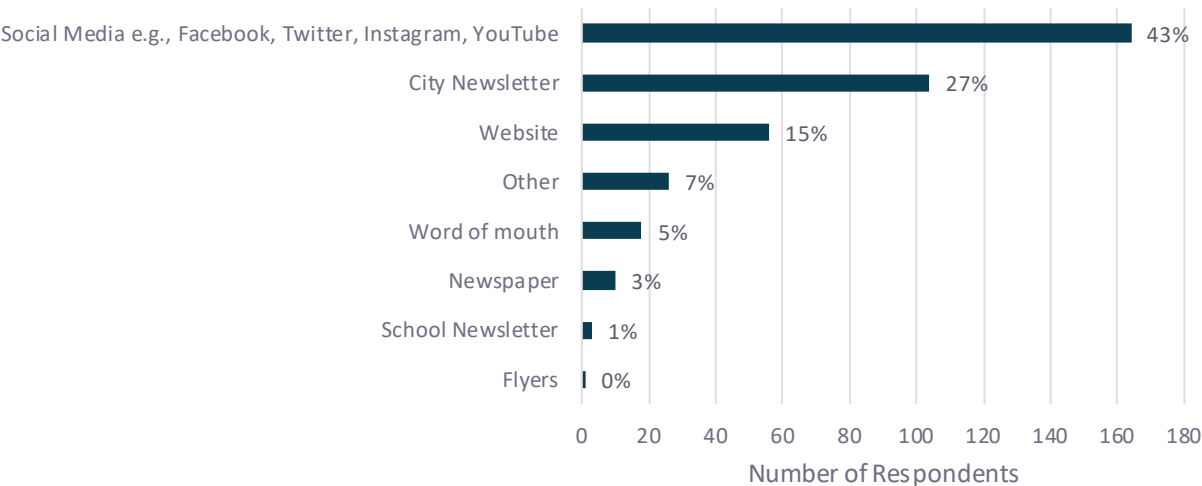
59% of respondents indicated they have two or more vehicles registered to their household. Less than 5% of respondents do not have any vehicles registered to their household.



n = 382

How did you hear about this survey?

43% of respondents heard about the survey through Langley City’s social media channels, including Facebook, Twitter, Instagram, and YouTube.



n= 382

MAPPING EXERCISE

The ESRI StoryMap included an exercise asking respondents to place pins on a map indicating a location with an issue or an opportunity. The points were categorized and included space to provide detailed comments. Respondents identified 534 points that are shown on the maps. The responses included strengths and weaknesses for bicycling, motor vehicles, transit, and walking.

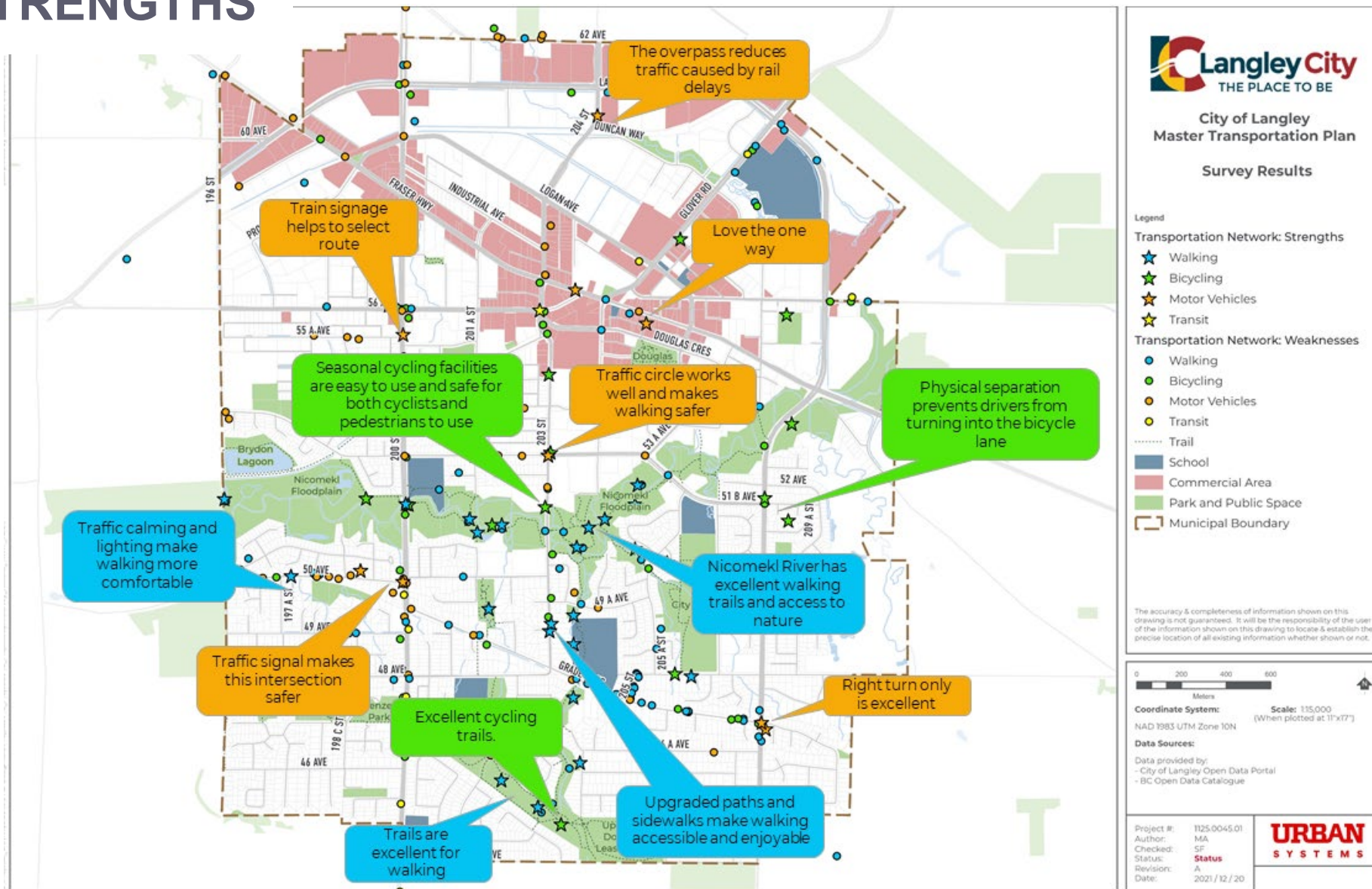
The maps on the following page highlight the most common comments within clusters of points.

MAPPING EXERCISE

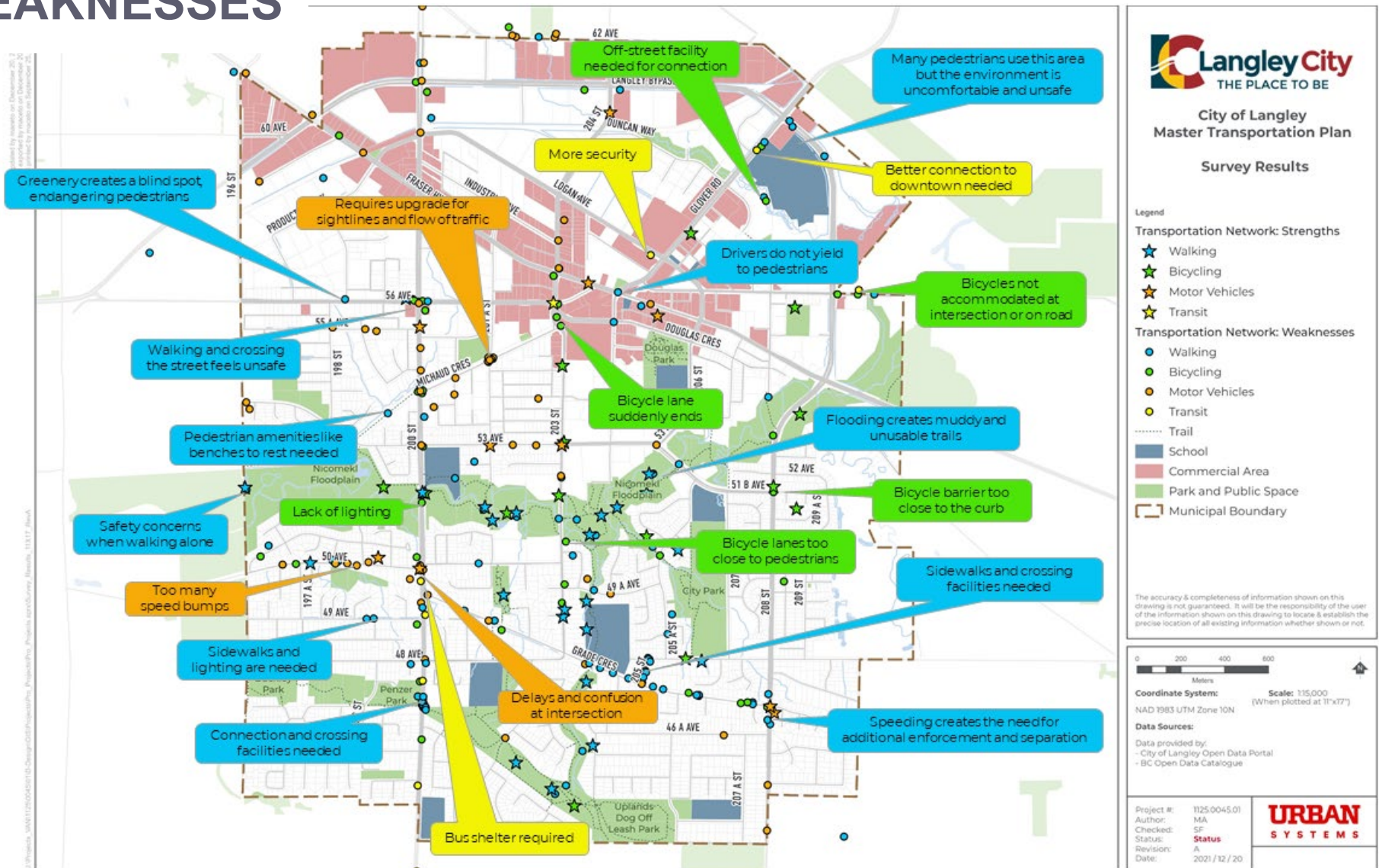
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The maps on the following page highlight the most common comments within clusters of points.

STRENGTHS



WEAKNESSES



SECTION 4: ONLINE PUBLIC CONSULTATION MEETINGS

Two 90-minute online public consultation meetings were held on Thursday, December 2nd at noon and 6pm via Zoom. These virtual sessions shared more details about the planning process and attendees were invited to share their ideas and experiences with travelling in Langley City. Event registration was hosted on EventBrite, and the events were promoted through the City's social media channels. Both sessions consisted of a presentation on the planning process and existing conditions and was followed by open group discussions.

Due to the on-going COVID-19 pandemic, the City chose to host these meetings virtually to ensure participants felt safe to join and participate. There were 22 registrations for the events, and a total of 10 participants attended the virtual meetings.

A presentation was delivered by the Langley City's Project Manager, Cameron Perkin, and the Consulting Team Lead, Sarah Freigang of Urban Systems Ltd. The presentation was followed by a question-and-answer session. A copy of the presentation is available in **Appendix A** and a summary of the Q&A is provided below.

Participant Comments

- I like the walkability of our community – most of the land is flat however there are some areas that are less pedestrian-oriented than others. When it comes to transportation, we need to look at more than vehicular traffic including, pedestrians, cyclists, buggies, and people in a wheelchair. I would like to enhance pedestrian-oriented nature of community.
- Cycling is a priority. I don't feel comfortable cycling, so want to make that more comfortable.
- Is Fort Langley part of the City? It is great walking, could be improved. The City is very confusing in terms of its relationship to the rest of the region. Noise is a problem. Violating provincial regulations with excessive noise. They make the environment unlivable. Langley tends to be a place where there are a lot of trucks – pay attention to this.
- I am concerned about safety. Nearly got hit in a crosswalk by someone who went through a red light. Are there new ideas to enhance pedestrian safety? No mention of commercial traffic. I noticed parking is not included.
- It would be good to have an overlay showing where ditch enclosures are needed to accommodate new sidewalk improvement. Also, where additional right-of-ways are needed to accommodate sidewalks and/or cycling lanes.
- I know budget and staff resources are always a challenge, but I think the size of Langley City is a good candidate to explore some Smart Cities transportation systems.
- It would be interesting to see through modeling if the extension of 53 Avenue to 196, right out or right in/right out, improve the LOS at Langley Bypass and other intersections.
- While we have made significant investments over the years to upgrade the traffic controllers to support synchronization, this is one area where more focus is required to enhance travel movement and flow along major arterial and collector roads.

- we are looking to the future. The significant impact of the SkyTrain to our small community will be a new era. There were no busses when I got here. That is changing now. People don't want to own their own cars anymore. The cost of the vehicle, insurance, maintenance, fuel, parking, etc is unaffordable. We need to look to the future.
- From a goods standpoint, drones are something that may need to be considered... as they get bigger and more able to drop stuff off. That provides an opportunity to restrict goods from a certain corridor because we allow flying.
- Active transportation collisions are of concern and trying to see how that fits into active transportation strategy. Good to see all that data is being pulled in.
 - building off comment on data, I serve on TransLink access users advisory committee. Have you considered or attempted to get access to HandyDart data? Fills an important transit gap. If a senior could walk from one block to the next, older adults could participate in active transportation.
 - Access to public washrooms are important. Appreciate all ages and abilities access to cycling routes. Cycling without age movement – accommodate larger bicycles that can go a long way. Scooters – new mobility which is great – but should include four wheeled mobility that older adults use. There are challenges for municipalities. Opportunity for collaboration.
- Something that is always occurring to me is developing a community shuttle service. Isolated pockets that have little to no real active transportation options. Have the nodes and styles, would like recommendations on how to improve. Would like more vehicles off the road. My feelings are that I wouldn't want to encourage automobiles. Appreciate participation of residents, businesses, and community partners to create a community we want to develop. This gives us an accurate picture of where we need to put the resources.
- Made me think of an alternate view of the user experience of how transportation occurs. Takes quite a while to bus – no rapid bus – user experience of how people get around. Yes, you don't walk, but why don't you walk? What are the issues of that? Sunny one day and not the next. But on Granville Island they try to create more covered sections for when its rainy.
- There is work with Langley senior society and conversations around community shuttles to the City of Langley that connects the Township.
- I love talking about active transportation and health of people – love seeing Fraser Health and seniors centre folks on the call.
- The City is looking forward with OCP, walkable and livable is the future for the City. Working to fight climate change, will want walkable and livable. Different transportation modes and how we come together. I would close off 204 to 206 St.

Questions to Participants

Q: What do you like most/least about transportation in Langley City?

A1: I am only week 10 into my role, our work space is right in the City. I like the availability of services right in the downtown core. Everything is fairly close from a services lens. I wish there was a direct express bus to get to work faster. But it's a lovely city.

A2: I think Langley City has everything to offer without big city problems. Parks are amazing, everything is so close. I don't like the number of obstacles in our walkways. I feel like I am always dodging a pole or hydrant. Sidewalks are in rough shape. Letdowns don't function the way they are intended to. Significant population in wheelchairs and scooters and disabilities and letdowns can be a point of frustration.

A3: Still getting to know the City as I'm new to the community. I love how the city has prioritized walking in many areas like paths, etc. Many do not prioritize walking in the same way. I am glad to see there is the commitment for bike lane improvements and would love to see more. I would like the plan to have an eye for active transportation and seniors, as they might have different transportation vehicles such as scooters. Car-centric nature of the city is a challenge and how does that work given the transit limitations or if someone is no longer able to drive, how do we support them especially with HandyDart not meeting needs. Sidewalk quality, sidewalk congestion.

Q: Did anything in the results stand out? What are your priorities?

A1: I am surprised public health is high up there on the results – that is good.

A2: I would have liked to have seen more interest in cycling. There's nothing worse than cycling and being in conflict with cars (parked and driving). Hoping that through COVID we would see more people walking and cycling and having more impact there. People are more conscious of being active. Pleasantly surprised that parking was lower. Hear about it quite a bit from a council position, new developments always saying too much or not enough.

A3: Nothing jumped out at me too much. Not surprised goods movement is so slow even though its so important. Nothing too shocking.

Participant Questions to Facilitators

Q1: When I look at the distribution of travel modes, less than 1/3 of trips are made by residents commuting to work. About 2/3 of trips are for pleasure or shopping. This may have a correlation between age distribution. Have you investigated that? Can we encourage people to use active and public transportation? One big factor is the economics of this and considering the age category. How practical is it to invite them to change their habits?

A1: As land use changes and access to destinations change, its easier to get to shopping, etc. If it's a short distance, people are more inclined to use different transportation choices. Cost is a factor for all ages – so is time. Creating a network that people feel comfortable and safe using, destinations are within proximity and making sure that it is efficient. Not just one mode will solve the problem – we need to look at all modes.

Q2: For 2km [trips], people are not going to use busses because they have to wait 30 mins.

A2: While there is a higher percentage of 65+, the majority is under 65. They can walk, bike, scooter slightly longer distances. There is an opportunity to encourage some of these trips. Not realistic to make everyone switch over. Even if there is a 5% shift, it reduces congestion on road network, and encourages a healthier lifestyle for a lot of people.

Q3: Future-looking: Will the MTP be considering AVs? Drone delivery?

A3: This is something that we have not yet considered, but is something we can look into and see if that type of technology is coming up. Electrification of the vehicle fleet is something we should look into. City of Kelowna has implemented this and had significant uptake.

Q4: Can you give more detail on complete streets and the methodology?

A4: It means reviewing, assessing and understanding the considerations for all modes and modes are best suited for a specific corridor.

Q5: I live in an area that is getting busy. Lived here 4-5 years, traffic is getting pretty scary. Concern is as we grow, connectivity, safety, how can that go smoothly? Langley POS-ABILITIES SOCIETY is working on an app to demonstrate the parts of the city that are accessible. Have they participated in the survey?

A5: As far as I know we haven't connected with them. Could reach out.

Q6: Would you please also talk about Vision Zero and how our MTP would include it in our plan?

A6: Vision Zero is something to look at. Through the process we will better understand whether or not council or residents would be interested in a policy like Vision Zero? Prioritize lowering speeds, making intersections safer, providing physical separation, eliminate severe and fatal collisions.

Q7: There are concerns about connectivity in terms of walking pathways or trails that connect various locations rather than taking the long walk around the block. Will this be part of the review process? Before development starts taking over and longer walks to reach a playground or park or trail or environmental component.

A7: There are a lot of opportunities to make connections through new developments and actions on connectivity as new development occurs.

Q8: Are there areas for pedestrian only?

A8: Within the city there isn't many areas for pedestrian only areas. But this could be Fraser between 204 and 206 St. This would require significant engagement buy in from businesses. Engagement was done in 2017 to reimagine the area, but could be something to explore in the future.

Online Messages & Feedback

In addition to the survey and online public consultation meetings, community members also communicated with the City through email and social media posts. We received two emails and 26 social media comments.

- Too much congestion and traffic issues (x10)
- Unhappy with prioritizing buses and bicycle lanes (x4)
- Do not understand why the City is already preparing for SkyTrain when it is not built yet (x2)
- Too much densification (x6)
- Would like to see the City become more walkable and be an attraction for locals (x1)

City Action Items

- Look at developing an overlay showing where ditch enclosures are needed to accommodate new sidewalk improvements and where additional right-of-ways are needed to accommodate sidewalks and/or bicycle lanes.
- Investigate continued improvements to traffic controllers to support synchronization and enhance travel movement and flow.
- Look into how drones and electric vehicles will impact commuting in the City.

SECTION 5: ONLINE STAKEHOLDER MEETING

An online stakeholder meeting was held on Thursday, December 9th at 4pm via Zoom. These session was specifically for community stakeholders and attendance was by invitation. Stakeholder organizations in attendance included:

- Kwantlen First Nation
- SD-35, Uplands Montessori
- Southern Railway
- Langley Community Services Society
- Fraser Health
- Greater Langley Chamber of Commerce
- Township of Langley
- Board Chair for Langley Senior Resources Society
- Kwantlen Polytechnic University (KPU)
- TransLink

The stakeholder meeting shared details about the MTP planning process and attendees were invited to share their ideas and experiences with travelling in the Langley City. The session included a presentation followed by an open group discussion.

The presentation was delivered by the Langley City's Project Manager, Cameron Perkin, and the Consulting Team Lead, Sarah Freigang of Urban Systems Ltd. The presentation was followed by an open group discussion. A summary of the discussion is provided below.

Group Discussion

Q1: Does your organization have any other plans, policies, and initiatives that we should be aware of?

- Kwantlen Polytechnic University
 - KPU2050 Official Campus Plan
- Township of Langley
 - MTP is from 2009, planning an update in 2022.
 - Willowbrook Community Plan update has been initiated.
 - Referenced the 2015 cycling plan but that also needs an update.
 - Climate Action Strategy.
- TransLink
 - Transit oriented affordable housing study.
 - New regional vision to advance goals from Transport 2050.
 - Regional Goods Movement Strategy.
 - TransLink is currently working on updating the goods movement network.
 - Bus speed reliability study for the region.
 - City of Langley is completing a bus speed and reliability study currently.

- Roberts Bank Trade Area Study looks at improvements to address connectivity that is lost when train goes through.
- The Chamber has been active with the Highway 1 improvement project.

Q2: Did anything in the results stand out? What are your priorities?

- Walking and accessibility is a priority for many seniors. The senior's centre has 1000 members and even more non-members.
 - We hear that many seniors travel on foot.
 - We hear more issues about walking.
 - We would like to see improved crosswalks with cut outs.
 - Several seniors are visually/hearing impaired at our organization, creating safety issues.
 - Happy to see walking and accessibility as a priority.
- It is important to ensure parents/guardians feel safe when walking and cycling with children. There should be a focus on building confidence on using active modes to and from school.
- It was noted that a lack of confidence could be why cycling scored low as a priority. There is a lack of confidence based on the existing network and feeling unsafe cycling.
 - With biking specifically, the City has limited protected cycling facilities. There soon will be more as we are building some multi-use paths but most current cycling facilities are painted bicycle lanes or shared use lanes.
- There's a lack of understanding of what goods movement means to the public. There is a gamut of service vehicles – they are never seen as a high priority, but goods movement is essential to the community.
- It was interesting to see driving as the number one priority – improving driving conditions is interesting and may not align with the City's and regional goals.

Q3: What is working well for walking, cycling and transit? What is not working well for walking, cycling and transit? How can we improve walking, cycling and transit?

- I would like the MTP to consider the purpose of trip and type of trip.
 - For example, there is a lot of cycling that is recreational.
- The Skytrain station at 196 Street is going to be important in the City of Langley even though it's located in the Township of Langley. It will be important to provide active transportation connections to the station and higher quality facilities, especially in the walking catchment.
- Off-street trails raise concern over lighting and being able to use them throughout the day.
- It is important to use caution when putting cycling facilities on low volume roads because parking can still be an issue.
- The plan needs to identify locations where sidewalks are needed.
 - There are no sidewalks on the Langley Bypass near Glover Road.
 - It was noted that the Langley bypass is owned by MOTI and the City does not have jurisdiction.
 - The section in reference was on City jurisdiction.
- Opportunities to provide active transportation connections to KPU.
 - Langley Bypass near Glover Road: Calm that area to align with the functional purpose of the road.
 - Natural space on the northeast side of Langley Bypass.
 - Involvement of bus integration plan with SkyTrain. Take people to all the places that SkyTrain doesn't.
- It was noted by TransLink that there will be a bus integration plan completed closer to opening day of SkyTrain.
 - TransLink noted that an area transport plan is being developed.
- There is an opportunity to look for ways to bring Indigenous perspectives into the plan.

- Previously, Kwantlen First Nation was not part of these discussions and is excited to be part of this plan.
 - Trails and multi-use pathways along the river are wonderful opportunities to share information and incorporate Indigenous public art opportunities, etc.
- As a plan there are two components: infrastructure and personal behaviour as it impacts personal safety? Will you be addressing personal behaviour? For example, driving unsafely in school zones. This will be important for building confidence and parents' willingness to let children walk to school.
 - There are a few different approaches – Safe Routes to School – which includes the three E's of engineering, enforcement, and education. This can include safe infrastructure and more lighting. There is a social/behavioural component that will likely need to be addressed as part of all City plans.
 - As far as infrastructure goes, Surrey has emergency stations where you can press a button to connect with a security guard, the City could look at those opportunities too.
 - There are also studies which suggest that there is increased safety by having more people using the facilities and walking/cycling. Safety in numbers.
- In terms of walking and cycling, one of the biggest challenges is continuity and connectedness. If a facility stops and starts it is very difficult to encourage people to try cycling.
- It would be good to see a flipping of investment priorities to walking, cycling, people-centred land use and transportation.
 - The City noted that the recent OCP update has identified that active modes are of higher priorities than vehicles.
- The plan should consider not only shifting mode choice (not everyone can walk/bicycle for all trips) but also think about other forms of transportation such as electric vehicles, carpooling and carshare. Anything that can reduce the use of gas powered vehicles.
 - The City is updating their Zoning Bylaw to include requirements around electric vehicle stations within new developments. For example, a certain percentage of units should have chargers or wires ready.

Q4: How might population in growth in Langley City impact your organization? What are your main concerns with the road network?

- Schools and the School District is trying to encourage students to walk. But policies are different for elementary, middle, and secondary schools.
 - Access to school areas is only going to get more congested with more development and population growth.
 - Working on implementing new schools in the City, but this process takes years and the demand is not yet here.
- We can encourage more active transportation, but businesses need to have access to goods and 90% of that moves on trucks. We cannot limit their access.
- Jobs are moving out to Abbotsford and Surrey which is going to result in an increase of traffic flowing through the City. We cannot control the through traffic.
- Need to consider the impact of people driving and parking at the SkyTrain station.
- The City noted that the City will be doing a parking study in the coming years. Pay and permit parking may be a recommendation to increase supply and turnover.
- The plan will be considering the incorporation of road safety policies like Vision Zero policies.
 - The next stage for the plan is a visioning process, where Vision Zero policies will be looked at.
- Growth in the Township of Langley and Surrey will have an impact on the City's transportation network. For example, Clayton and Surrey. This MTP should consider how we accommodate those trips that will enter the City but may not be residents.
- Will a truck route network review be part of the MTP update?

- Yes, it will be. Falls into the goods movement piece. TransLink is also working on updating their regional goods movement strategy.
- Consider that school buses with much longer wheelbase need to use the streets. For example, traffic circles are too narrow for school buses.
- Eliminating parking is not a solution. Need to think about elders and accessibility.
 - It will be important to look at a variety of strategies and options for everyone.
- TransLink noted that the region is shifting priorities to more active and sustainable transportation. A key piece is to look at maximizing people moving capacity.

Action Items from Meeting

- If any other plans, policies, and initiatives come to mind please follow up with Cameron (cperkin@langleycity.ca)
- City asked if School District could send the community bus routes and length of the school bus to be considered in the plan.
- The project team provided a link to an interactive note board where stakeholders can add additional comments and share with others from their organization – <https://app.mural.co/invitation/mural/urban systems3772/1637347827339?sender=license3702&key=b8f0283c-5905-45a2-b00d-6e7dcc9c48d1>

APPENDIX A: ONLINE PUBLIC CONSULTATION PRESENTATION



MASTER TRANSPORTATION PLAN

VIRTUAL COMMUNITY EVENT

December 2, 2021





AGENDA

1. WELCOME AND INTRODUCTIONS

2. PLAN OVERVIEW

3. TRANSPORTATION IN LANGLEY TODAY

- WALKING
- CYCLING
- TRANSIT
- DRIVING

4. DISCUSSION

5. NEXT STEPS



MEETING OBJECTIVES



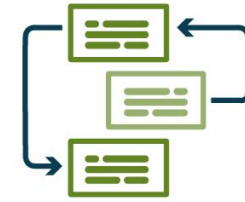
To introduce community members to the project and team



To share the results of the existing conditions analysis that will inform the Plan



To build a common understanding of key transportation issues and opportunities



To understand what is important to you and what you would like to see in the Master Transportation Plan



KEY QUESTIONS FOR TODAY

- What are the key issues and opportunities for Langley City today and in the future?
- What key improvements are needed to Langley City's transportation system?
- What does a successful plan look like?



ZOOM HOUSEKEEPING

Rename Yourself

Audio and Video Settings

Raise a Hand

View Participants
and Chat Window

Mute Stop Video

Participants Chat Share Screen Record Reactions

Leave



TELL US ABOUT YOU!

TYPE IN THE CHAT...

- Your name
- Why the Master Transportation Plan is important to you?





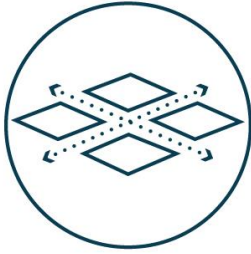
PLAN OVERVIEW

WHAT IS A MASTER TRANSPORTATION PLAN?

- The Master Transportation Plan will help guide the development of a *multi-modal transportation network* that provides *safe* and *efficient* movement of people and goods by all modes.
- A plan for everyone – a balanced plan that meets the needs of all road users



WHY IS TRANSPORTATION IMPORTANT?



Land Use



Applies to
Everyone



Public Health



Sustainability



Manages
Congestion



Equity



WHAT WILL BE IN THE PLAN?

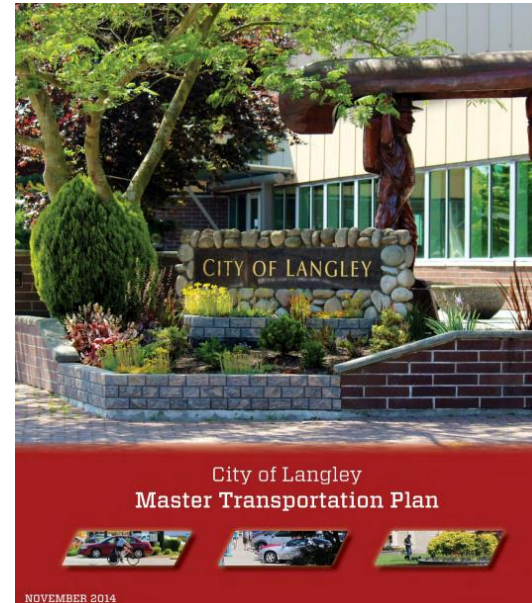
- A long-term **vision** for transportation in the City of Langley
- Goals and **objectives** that support the vision
- Long-term **plans** for different transportation modes
- **Priorities** for implementation and costs



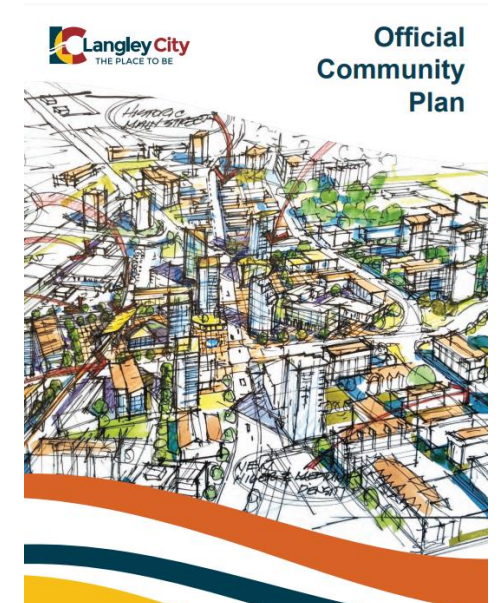
WHY IS AN UPDATED PLAN NEEDED?

- The Master Transportation Plan will help guide the development of a *multimodal transportation network* that provides *safe* and *efficient* movement of people and goods by all modes.
- A plan for everyone – a balanced plan that meets the needs of all road users

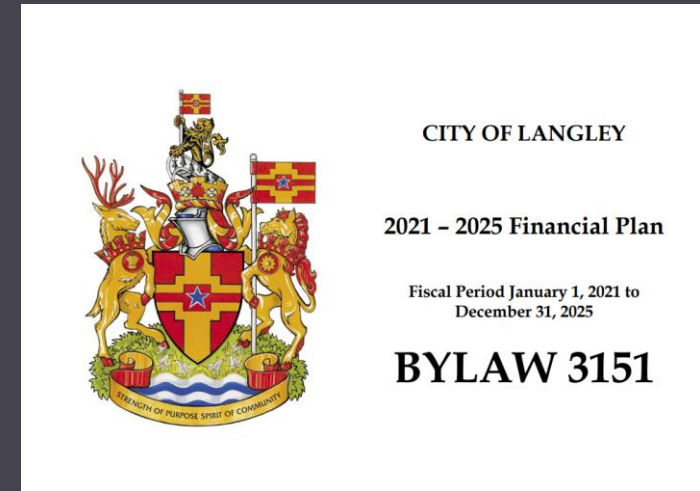
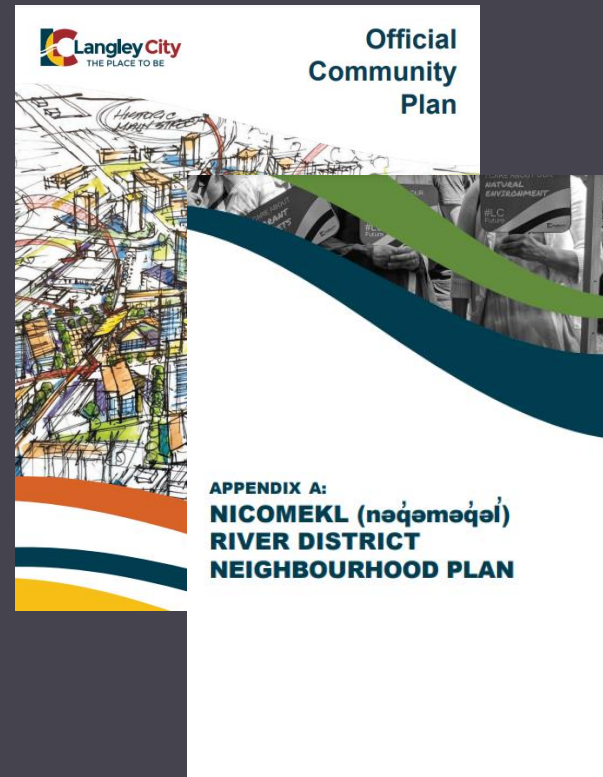
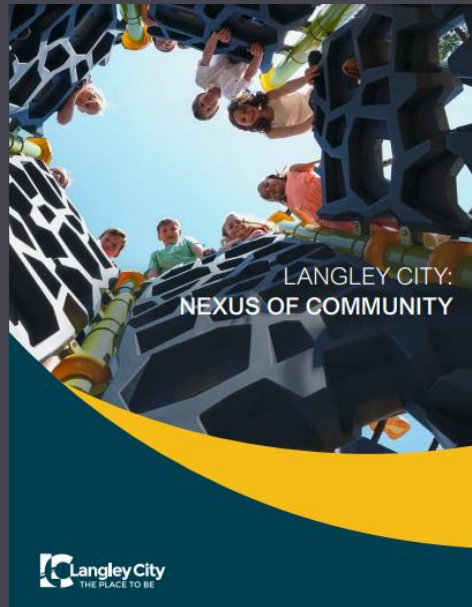
2014



2021



RELATED INITIATIVES



STUDY PROCESS



TYPE IN THE CHAT...

- What do you like **most** about moving around Langley City today?
- What do you like **least** about moving around Langley City today?



A light gray background featuring a complex, stylized map pattern of streets and blocks.

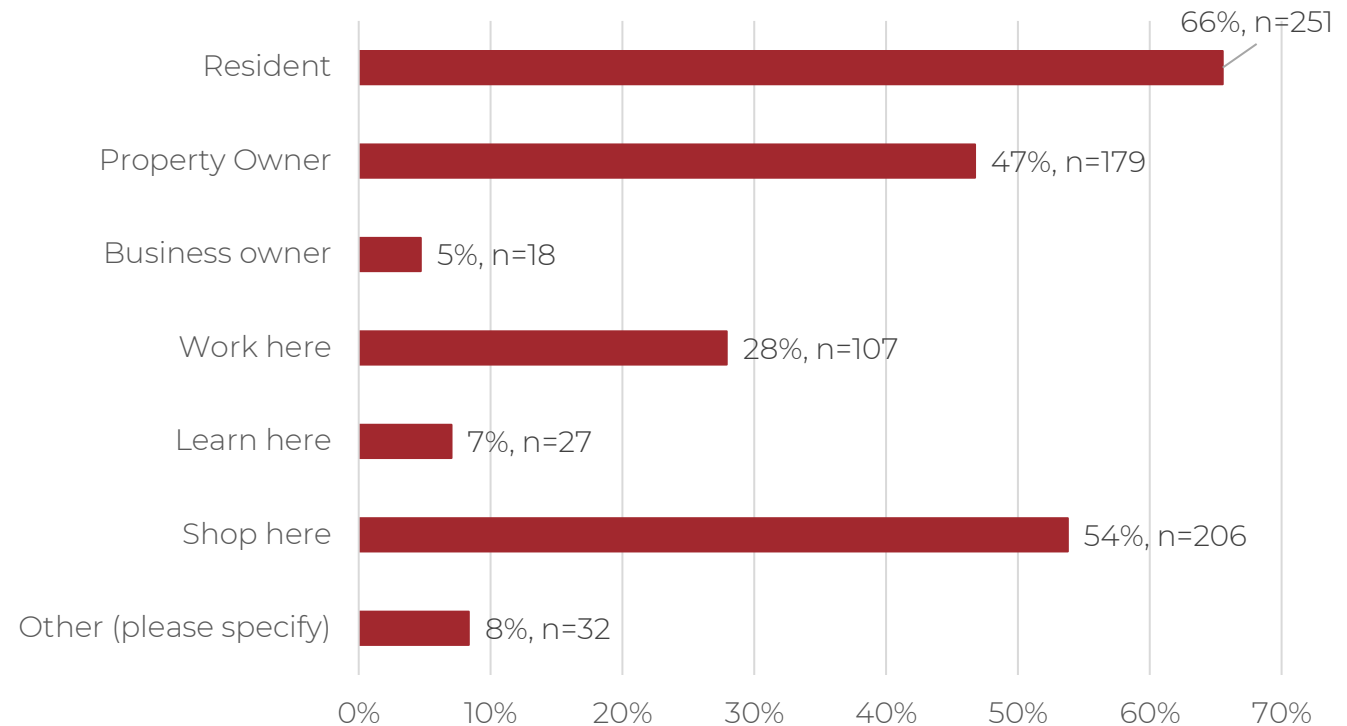
EXISTING CONDITIONS OVERVIEW

WHAT WE'VE HEARD TO DATE

COMMUNITY SURVEY

- Open from October 27-November 29
- Received 426 responses
- 66% of respondents identified as residents of Langley City
- 54% shop in Langley City

What is your connection to City of Langley? (n=383)

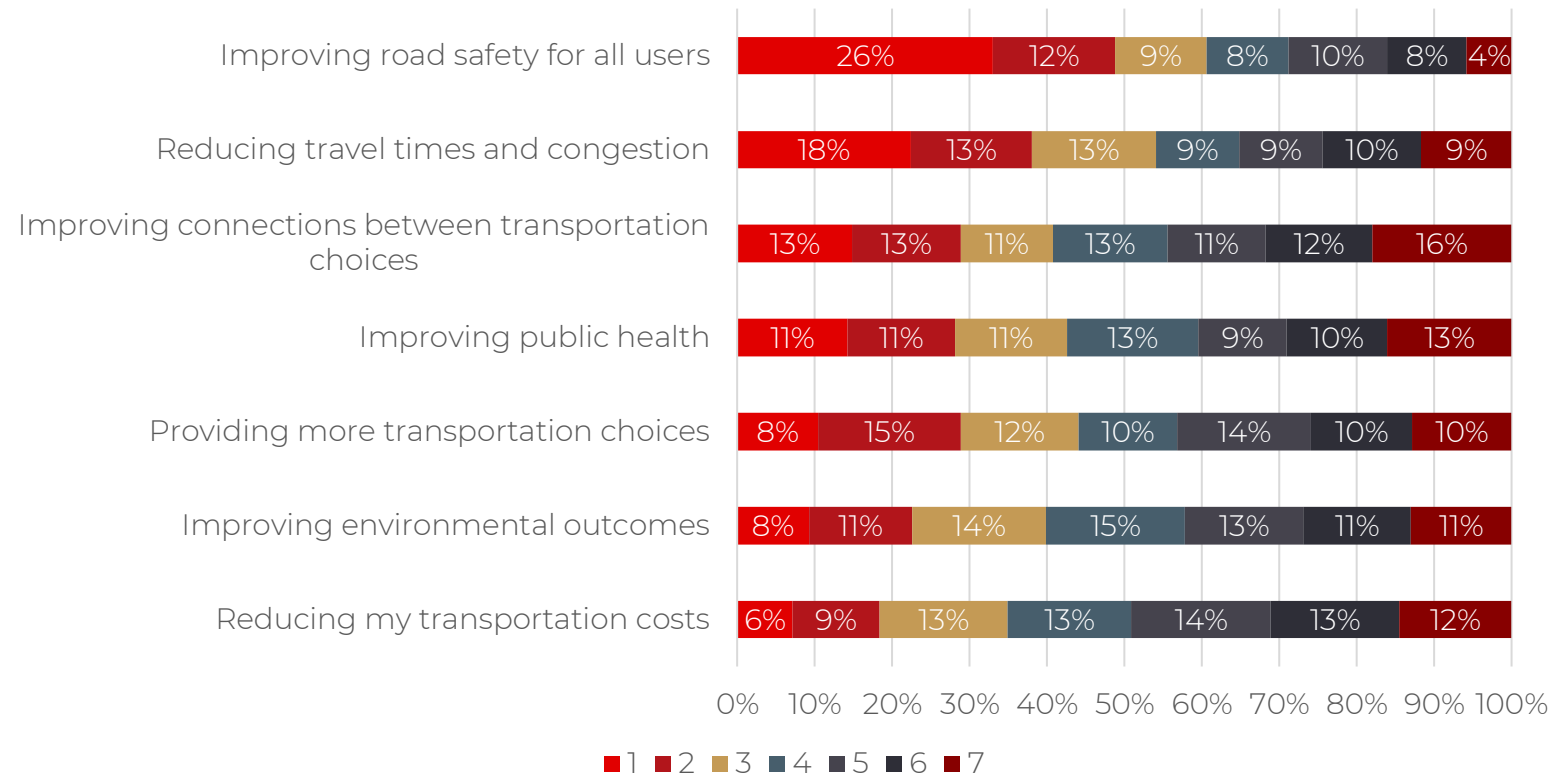


WHAT WE'VE HEARD TO DATE

WHAT IS IMPORTANT TO YOU?

- Improving road safety for all users was ranked the most important outcome of the Master Transportation Plan

Which of the following outcomes are most important to you? (n=423)

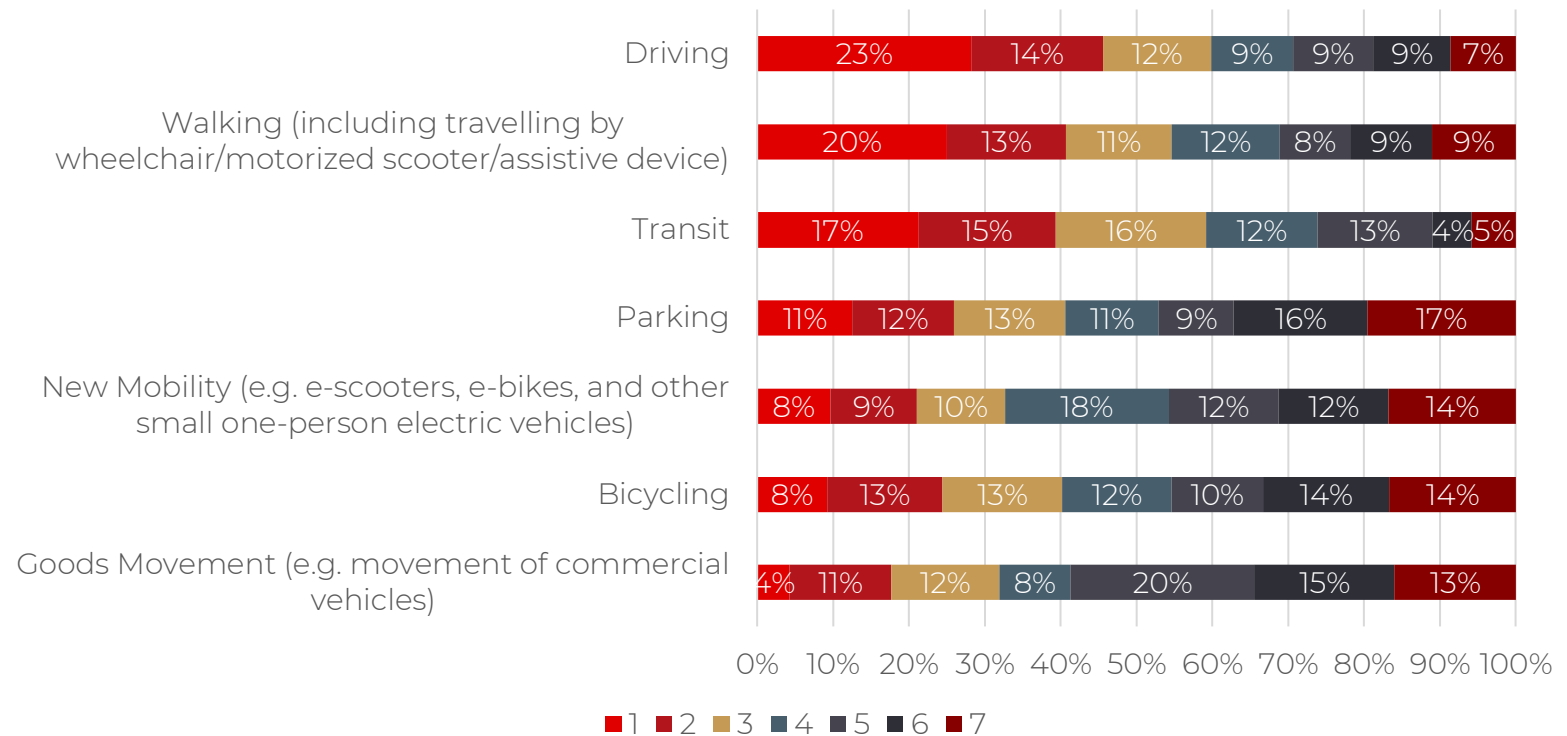


WHAT WE'VE HEARD TO DATE

HIGHEST PRIORITY?

- Respondents identified **driving, walking, and transit** as the highest priorities

What aspects of the City of Langley's transportation system should be considered the highest priority? (n=423)



DISCUSSION

- Did anything in the results stand out?
- What are your priorities?

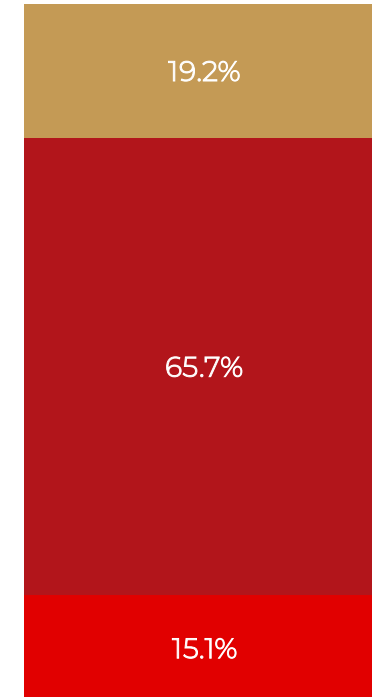


COMMUNITY CONTEXT

DEMOGRAPHICS

- The median age in Langley City is 42.2 – higher than the regional average of 40.9
- Median income is \$33,658/year – 17.3% are low-income
- The first language of 15% of the population is something other than English or French

AGE PROFILE



■ 0 to 14 ■ 15 to 64 ■ 65+

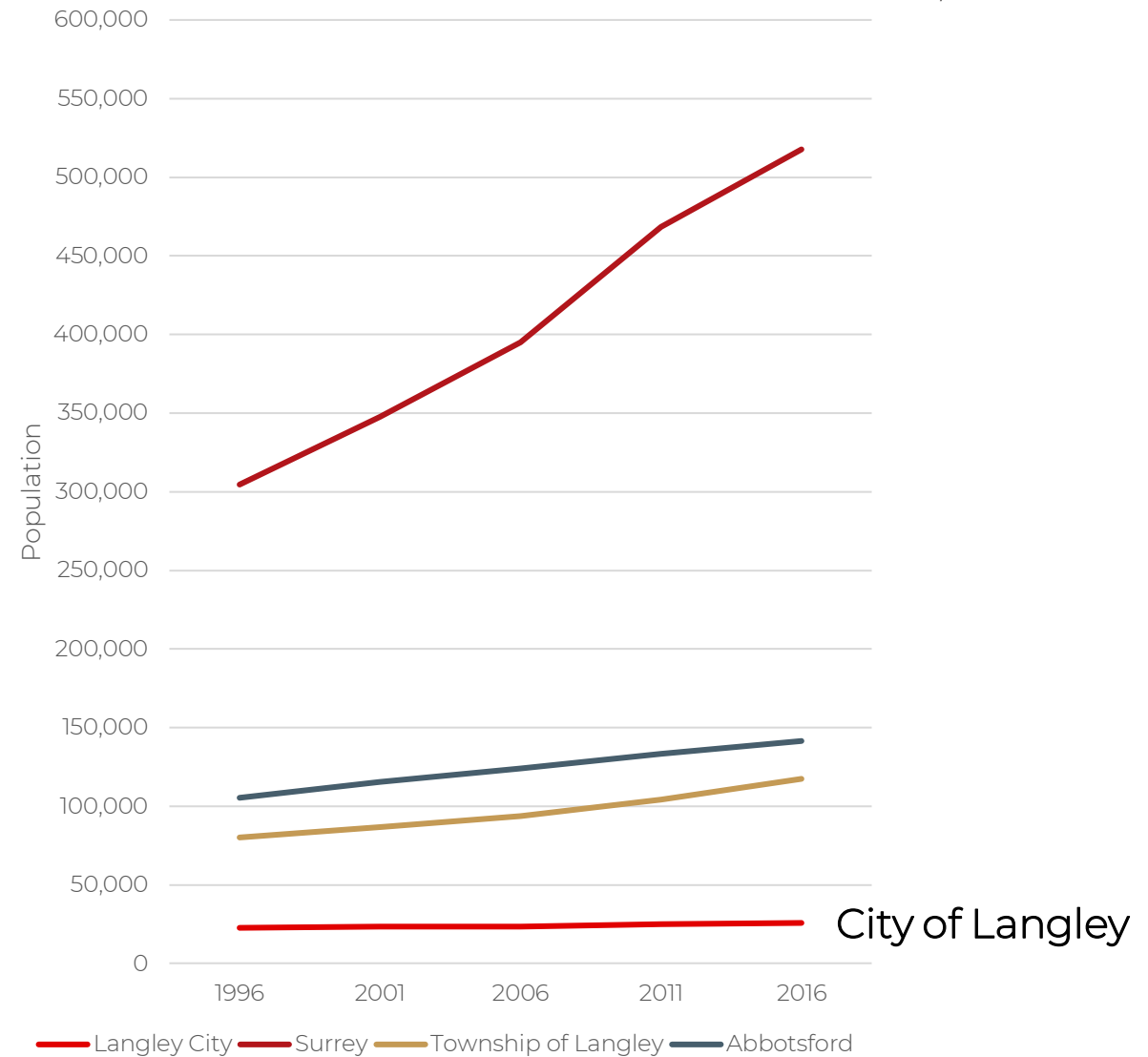
Source: Statistics Canada Census, 2016

COMMUNITY CONTEXT

POPULATION GROWTH

- 13% population growth since 1996
- Growth has been slow
- New developments: expect 25% population increase in coming years
- Average household size is 2.1 – lower than the regional average of 2.6

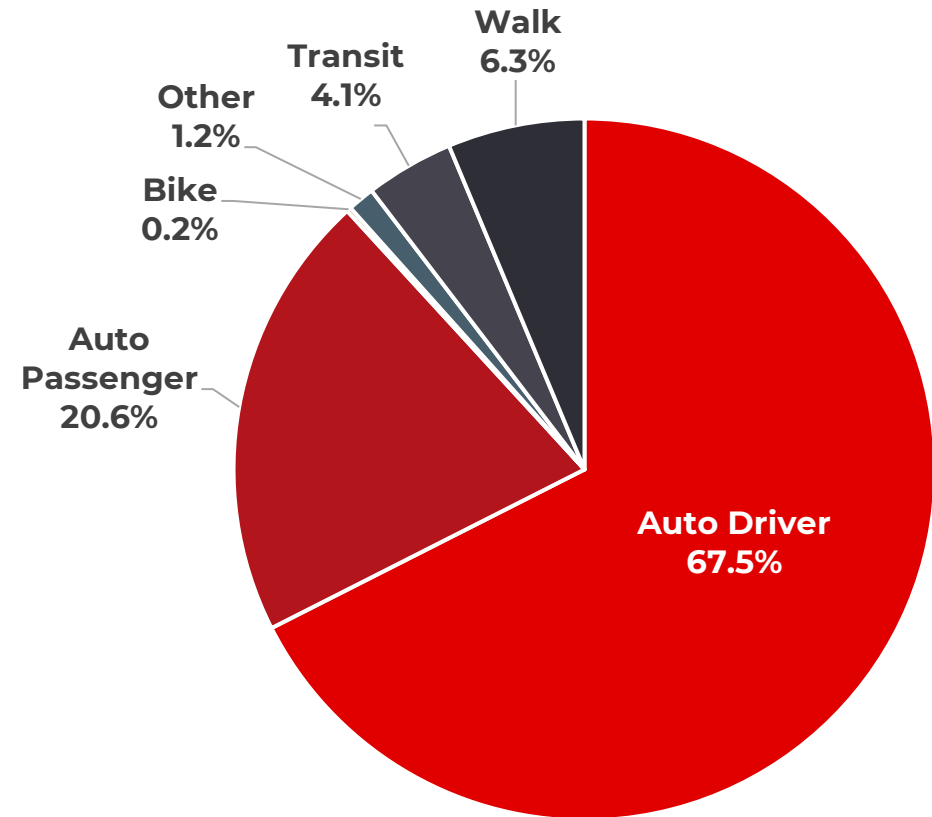
Source: Statistics Canada Census, 1996-2016



TRAVEL PATTERNS

HOW WE TRAVEL

- Approximately 88% of all trips are made by vehicle
- Sustainable transportation makes up nearly 12% of all trips
- Mode share has stayed relatively the same since 2008, with walking increasing by 3%

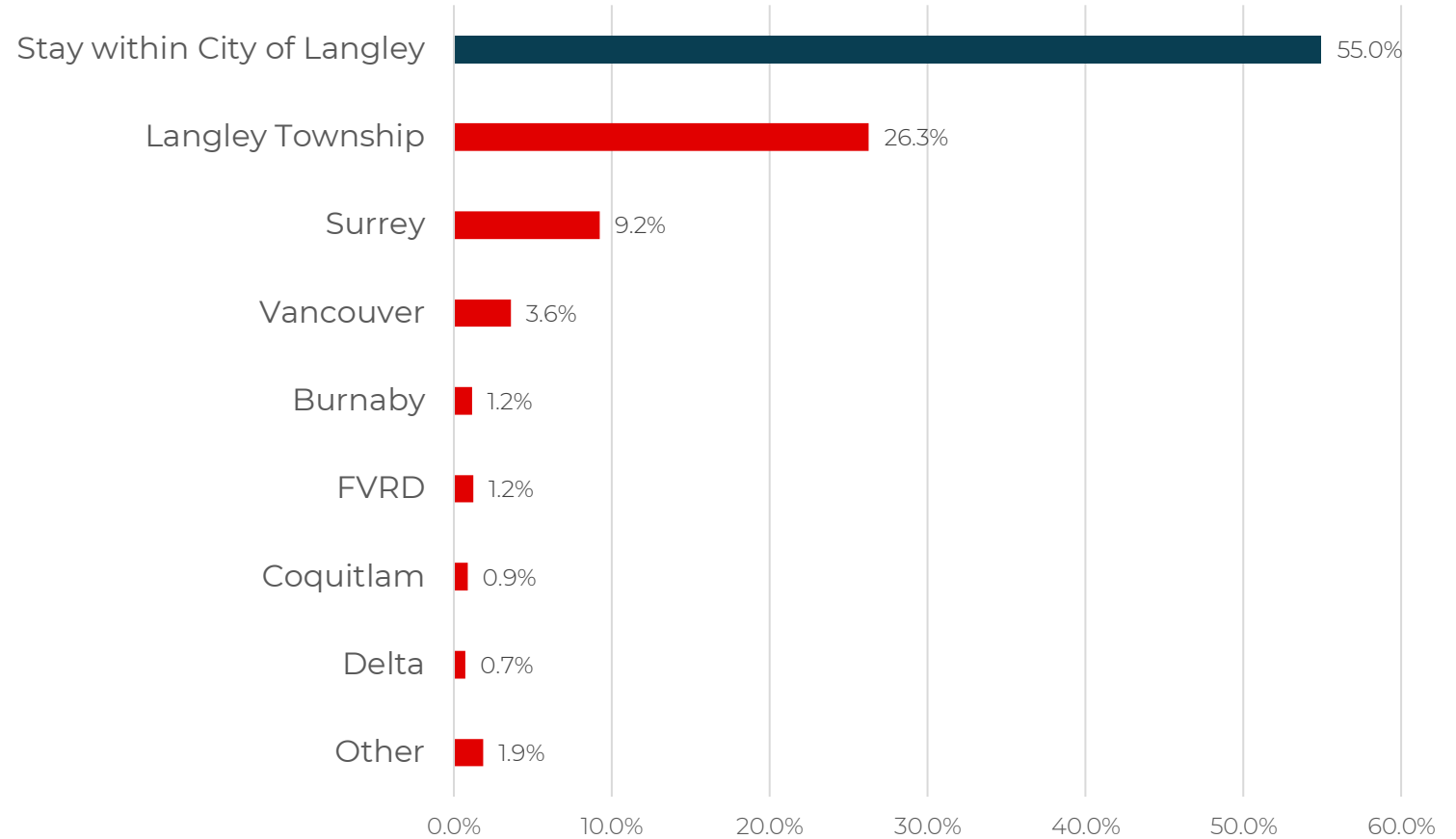


Source: TransLink Trip Diary, 2017

TRAVEL PATTERNS

WHERE WE TRAVEL

- Most trips generated by Langley City residents stay within Langley City

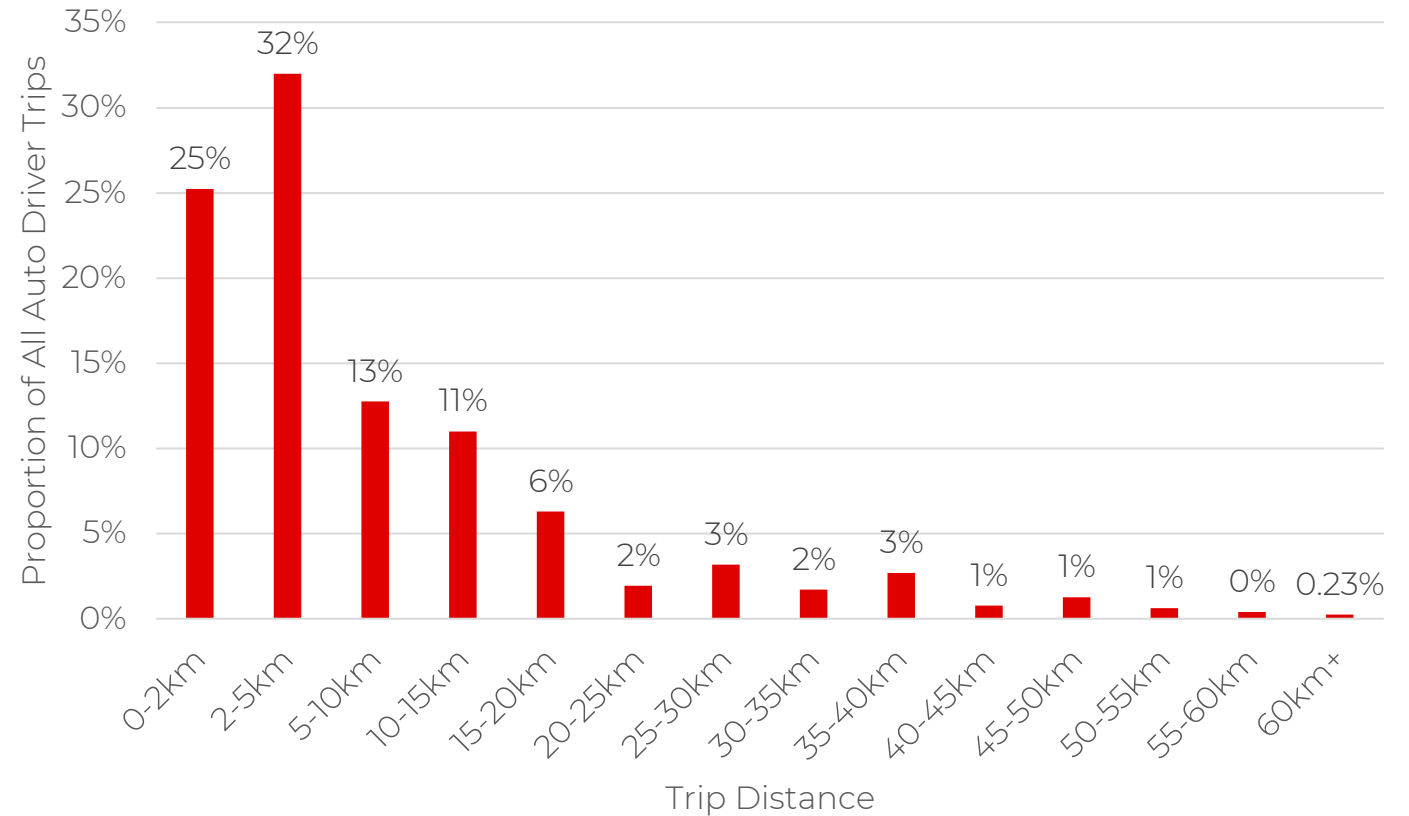


Source: TransLink Trip Diary, 2017

TRAVEL PATTERNS

HOW FAR WE TRAVEL

- 57% of all driving trips are less than 5 km, including 25% that are 2 km or less, a distance which could be replaced by active transportation



Source: TransLink Trip Diary, 2017

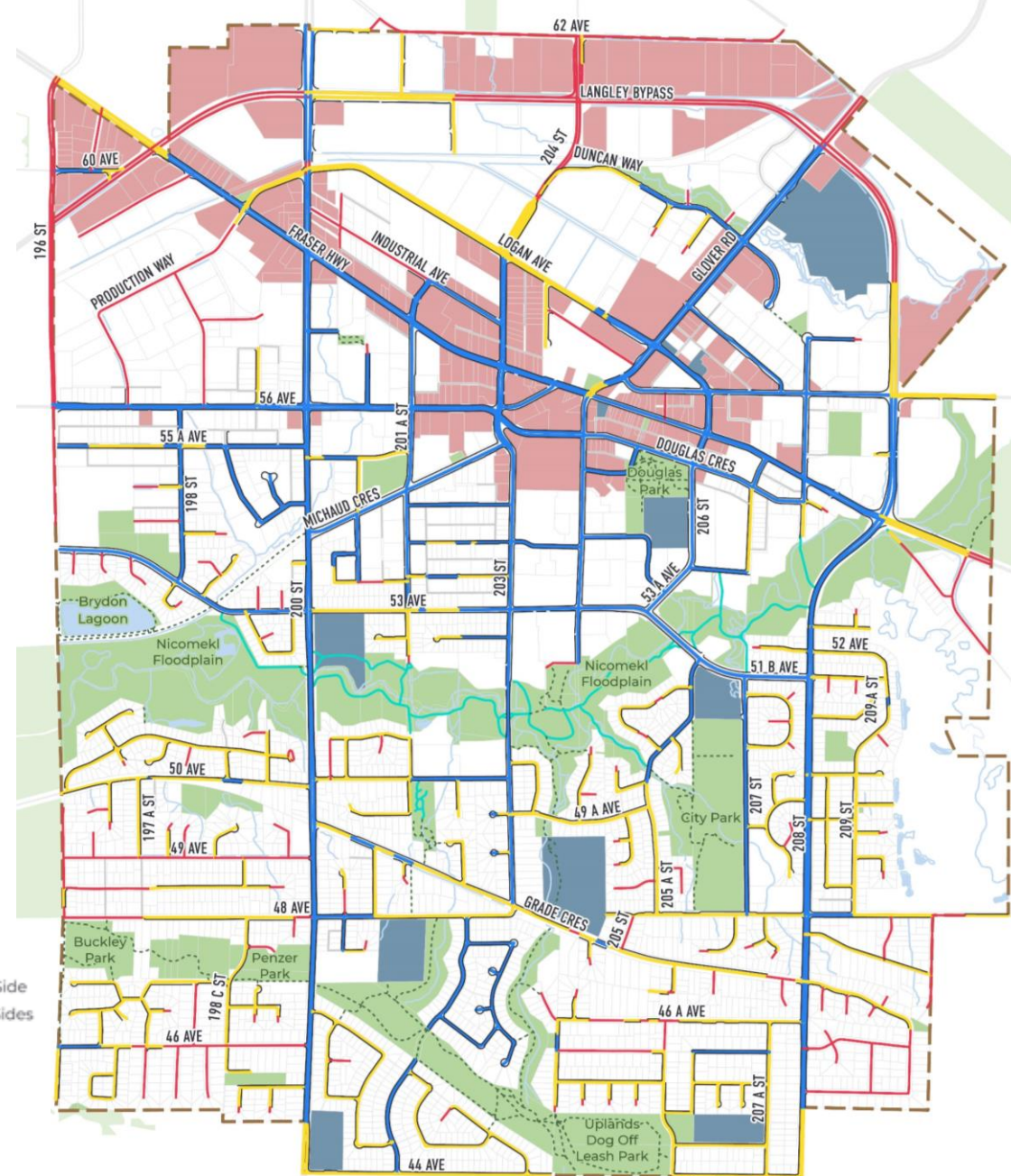
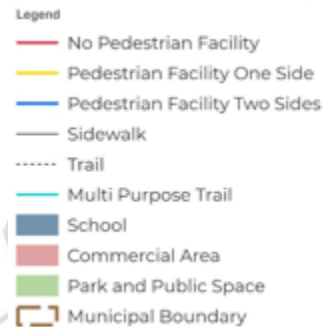


WALKING & ACCESSIBILITY

WALKING & ACCESSIBILITY

FACTS AND FINDINGS

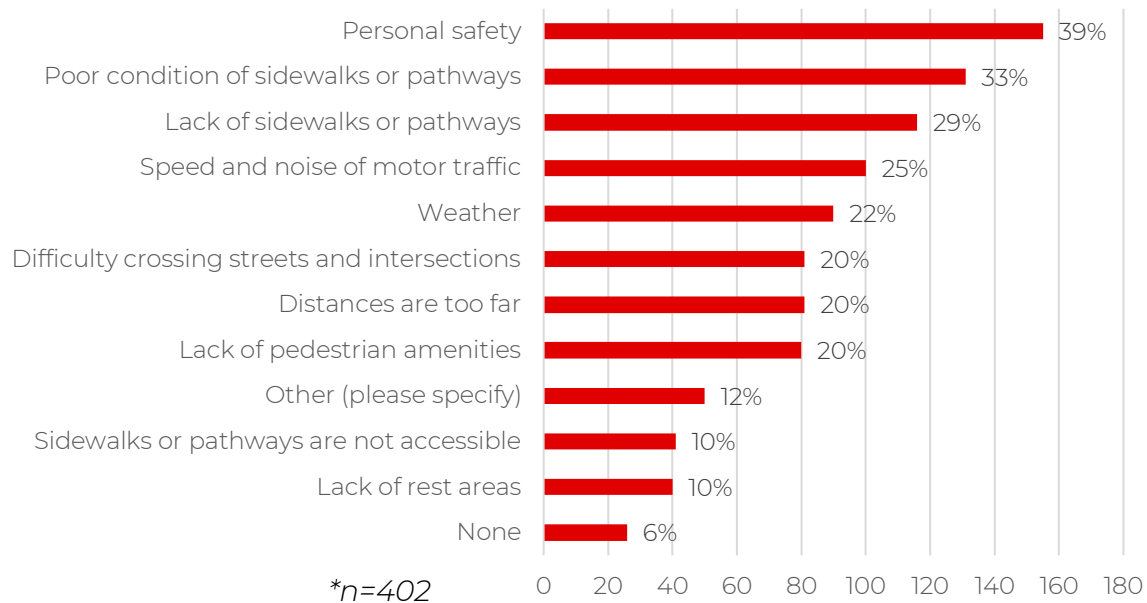
- Walking makes up 6.3% of all trips
- 29% of streets do not have a sidewalk on either side
- 37% of streets have sidewalks on both sides
- Most gaps in the sidewalk network are on local and arterial streets



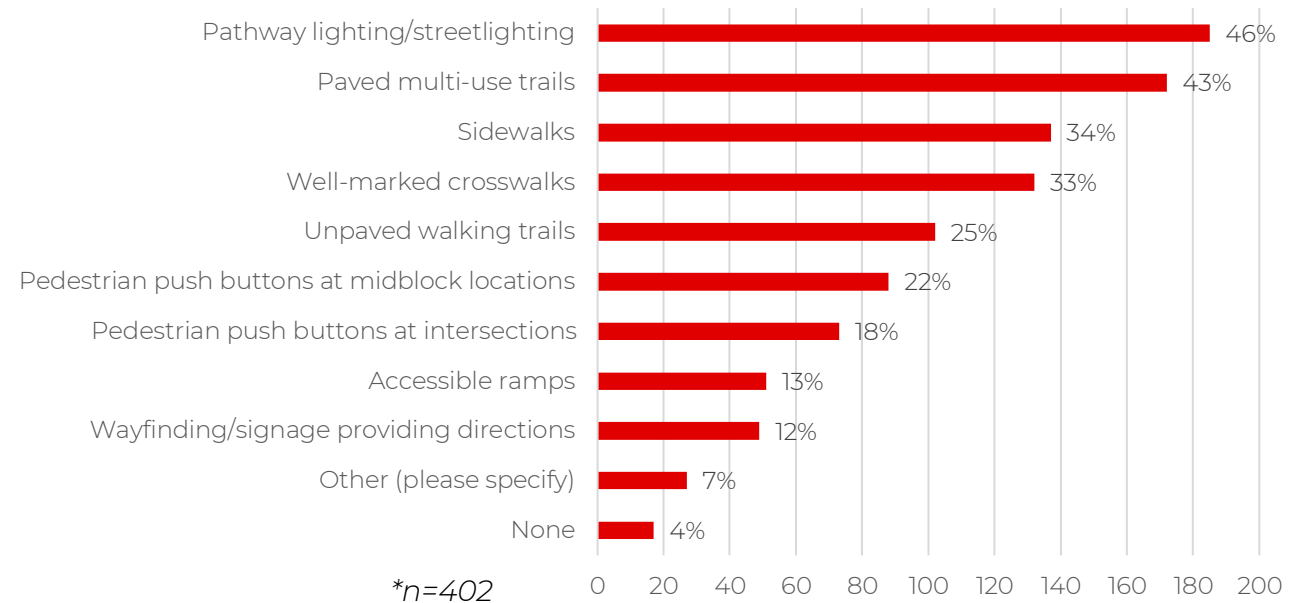
WALKING & ACCESSIBILITY

ISSUES AND OPPORTUNITIES (COMMUNITY SURVEY)

ISSUES



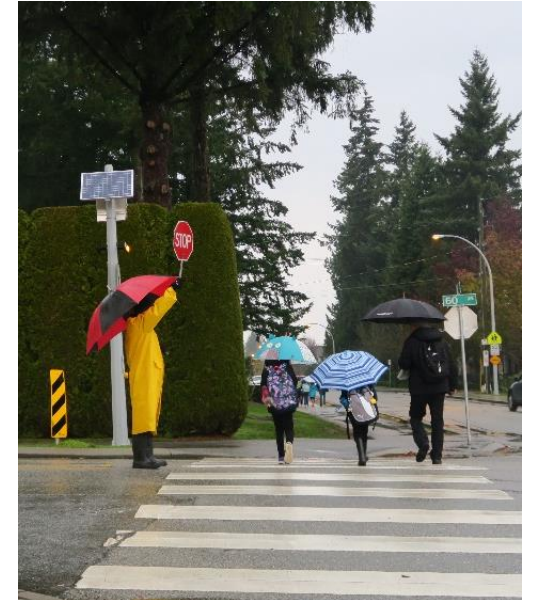
OPPORTUNITIES



WALKING & ACCESSIBILITY

TRENDS AND OPPORTUNITIES

- Safety and crossing improvements
- Safe Routes to School
- Tactical urbanism



ACCESSIBILITY

ACCESSIBILITY

- Designing infrastructure and the pedestrian environment for all ages and abilities
- Considering a wide cross-section of users: people with physical or cognitive disabilities, seniors, families with children



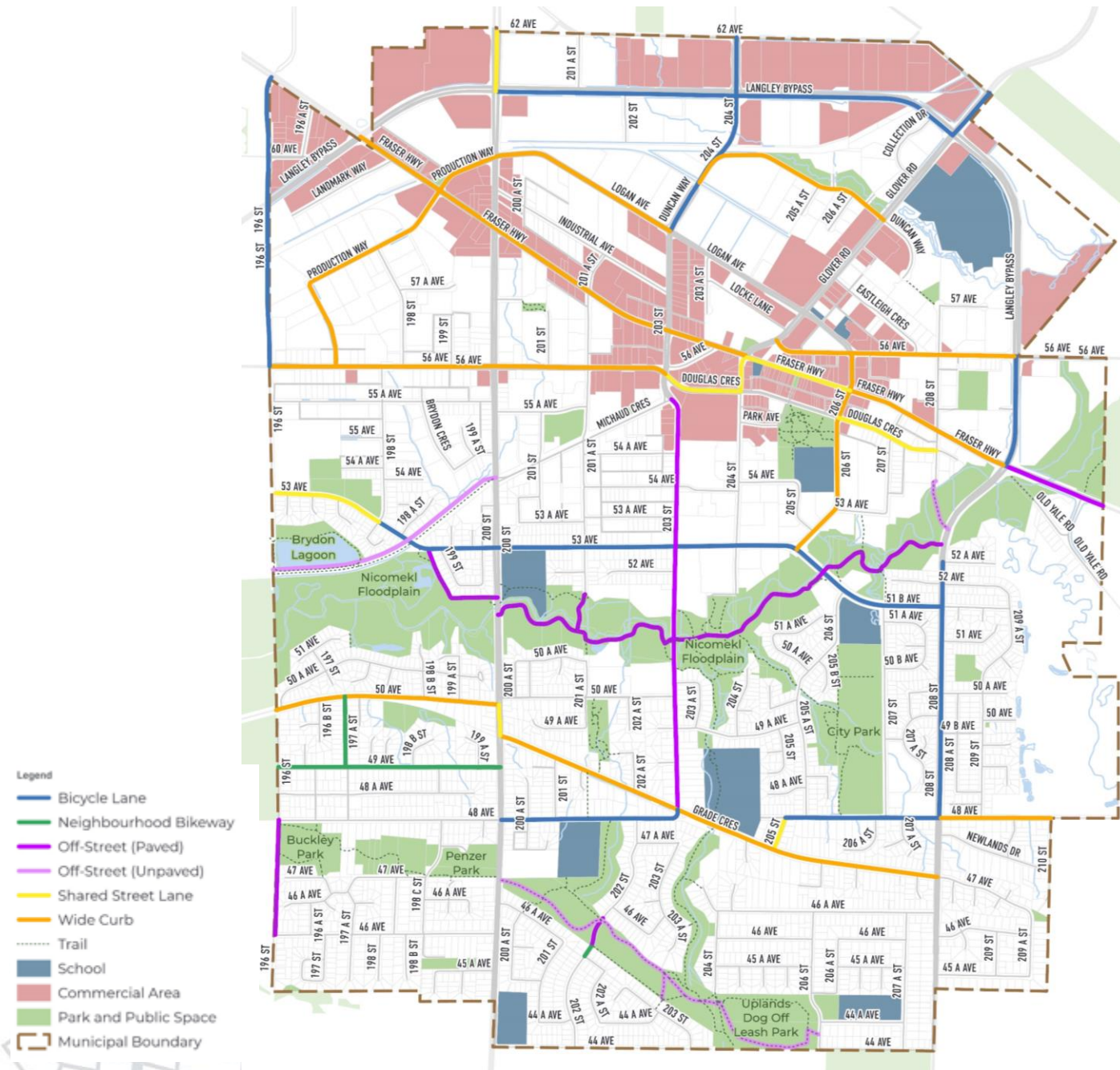


CYCLING

CYCLING

FACTS AND FINDINGS

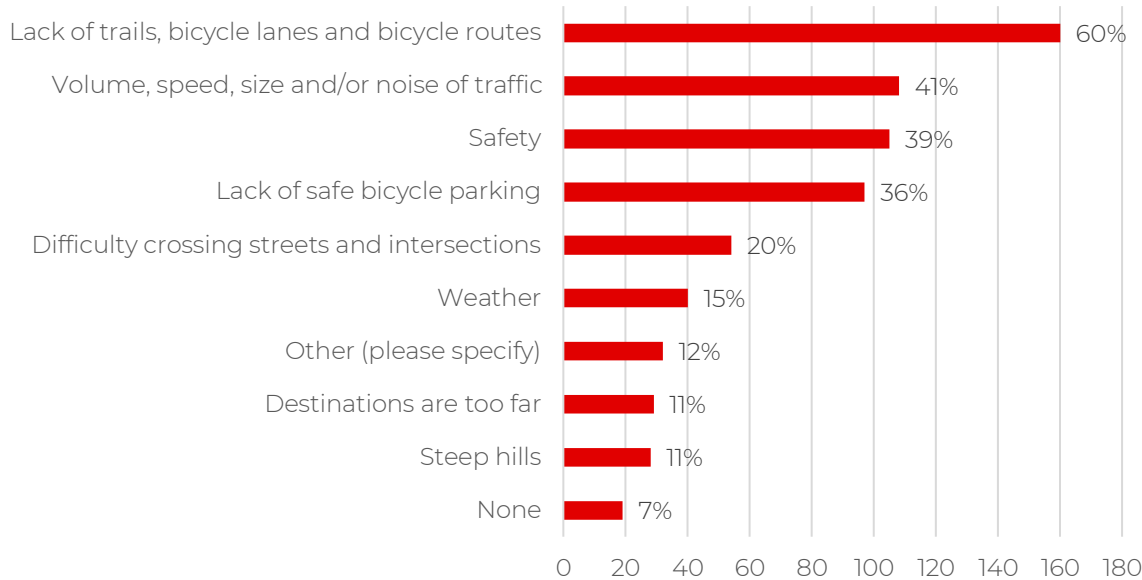
- Cycling makes up less than 1% of all trips
- 49.02 km of cycling infrastructure
 - 29 km of bicycle lanes, neighbourhood bikeways and shared street lanes
 - 9.5 km of multi-use pathways and trails (off-street paved and unpaved)



CYCLING

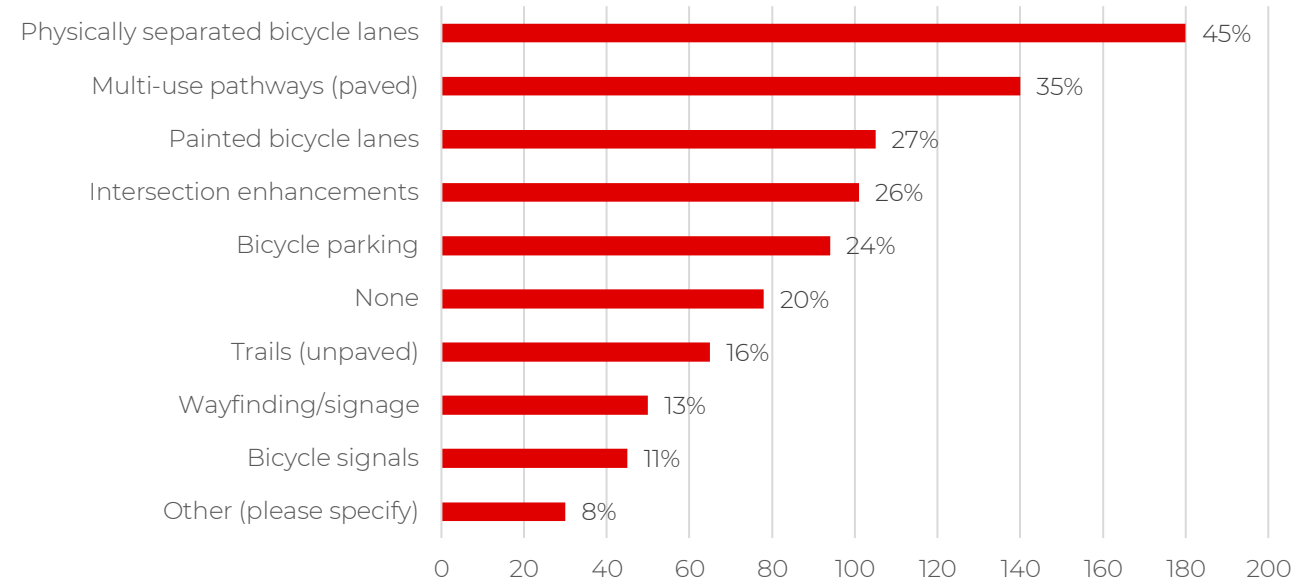
ISSUES AND OPPORTUNITIES (COMMUNITY SURVEY)

ISSUES



*n=266, 130 respondents identified they do not bicycle.

OPPORTUNITIES



*n=396

CYCLING

TRENDS AND OPPORTUNITIES

- All Ages and Abilities (AAA) network
- Rapid implementation strategies
- Planning for e-bikes
- End-of-trip facilities
- Micromobility
- Open streets

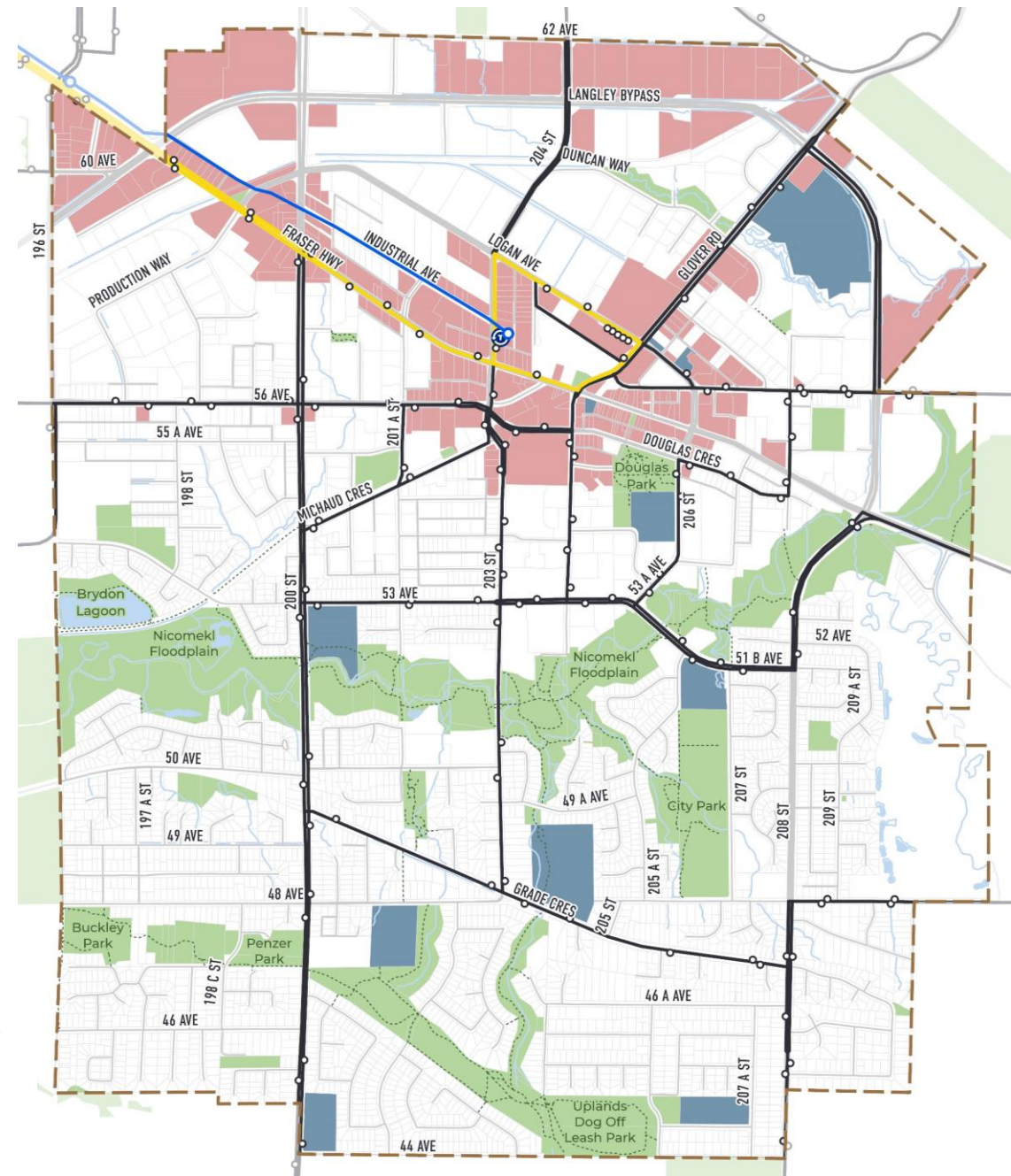




TRANSIT

FACTS AND FINDINGS

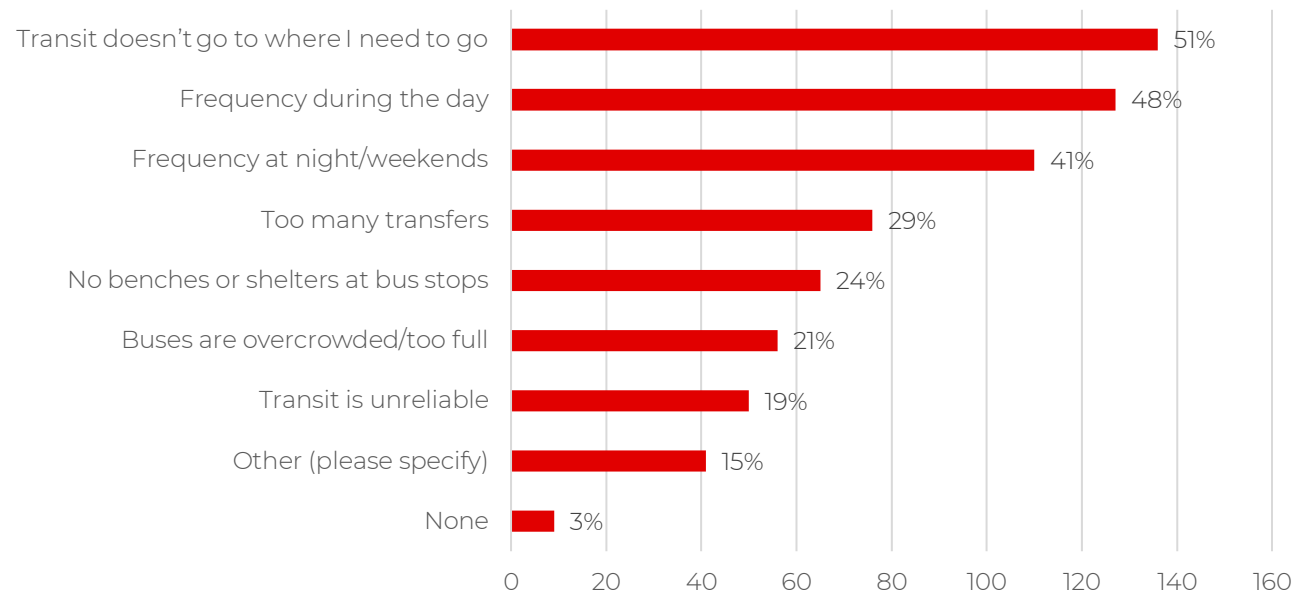
- Legend**
- Future SkyTrain Line
 - Transit Exchange
 - Conventional Bus Route
 - Frequent Transit Network Bus Route
 - Trail
 - School
 - Commercial Area
 - Park and Public Space
 - Municipal Boundary



TRANSIT

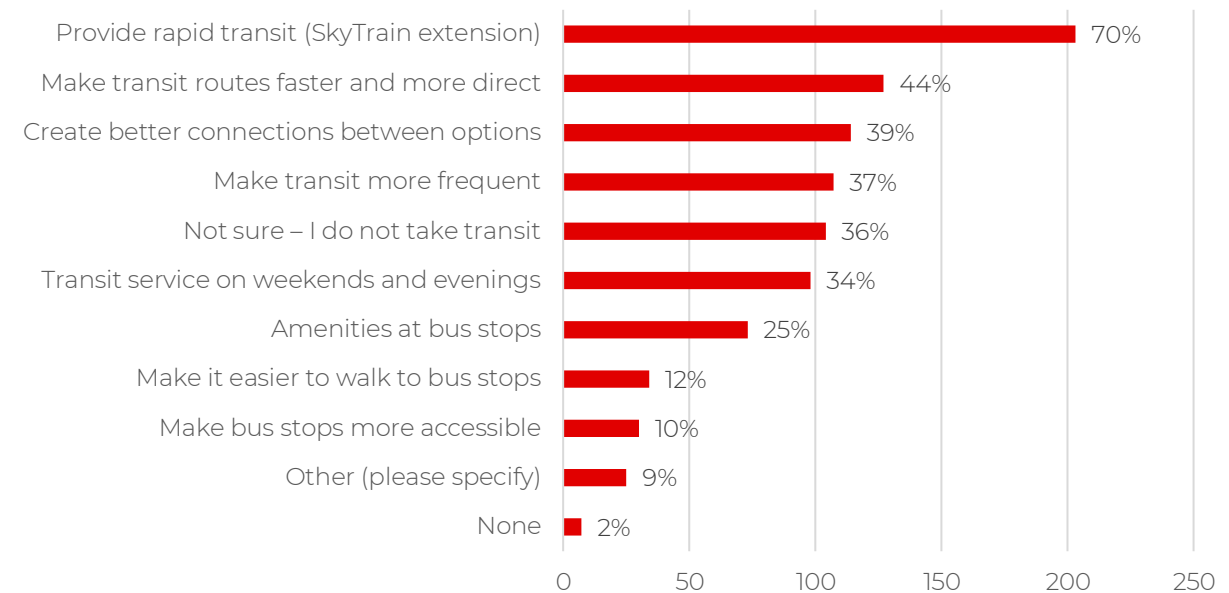
ISSUES AND OPPORTUNITIES (COMMUNITY SURVEY)

ISSUES



**n=266, 143 respondents identified they do not use transit*

OPPORTUNITIES



**n=291, 150 respondents identified they do not use transit*

TRANSIT

TRENDS AND OPPORTUNITIES

- Transit optimization to increase bus speed and reliability
- First and last kilometer: multi-modal integration with pedestrian and cycling networks

TransLink Control	OPERATIONS	Stop Relocation or Consolidation	Boarding Policy	Route Design
Municipality and MOTI Control	SIGNALS	Turn/Movement Restrictions	Queue Jumps	Transit Signal Priorities
		Bus-only Signals	Signal Phase Modification	
	INFRASTRUCTURE	Bus Platform Design	Bus Bulges	Boarding Islands
		Roadway Channelization	Parking Removal	Turn Radii Improvements
	TRANSIT LANES	Curb-side Bus Lanes	Interior/Offset Bus Lanes	Median Bus Lanes
		Contraflow Bus Lanes	Queue Bypass/Transit Approach Lanes	



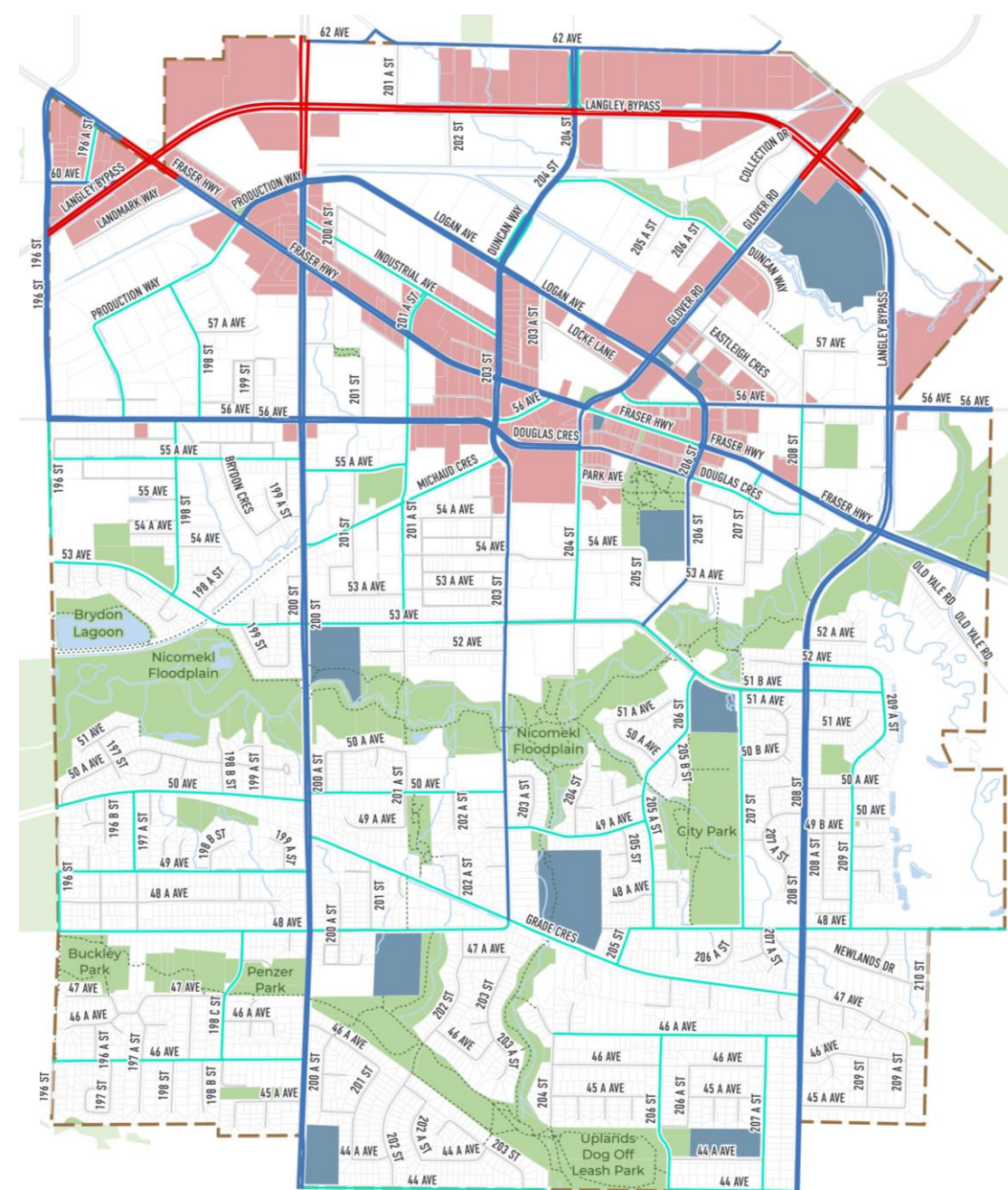


DRIVING

DRIVING

FACTS AND FINDINGS

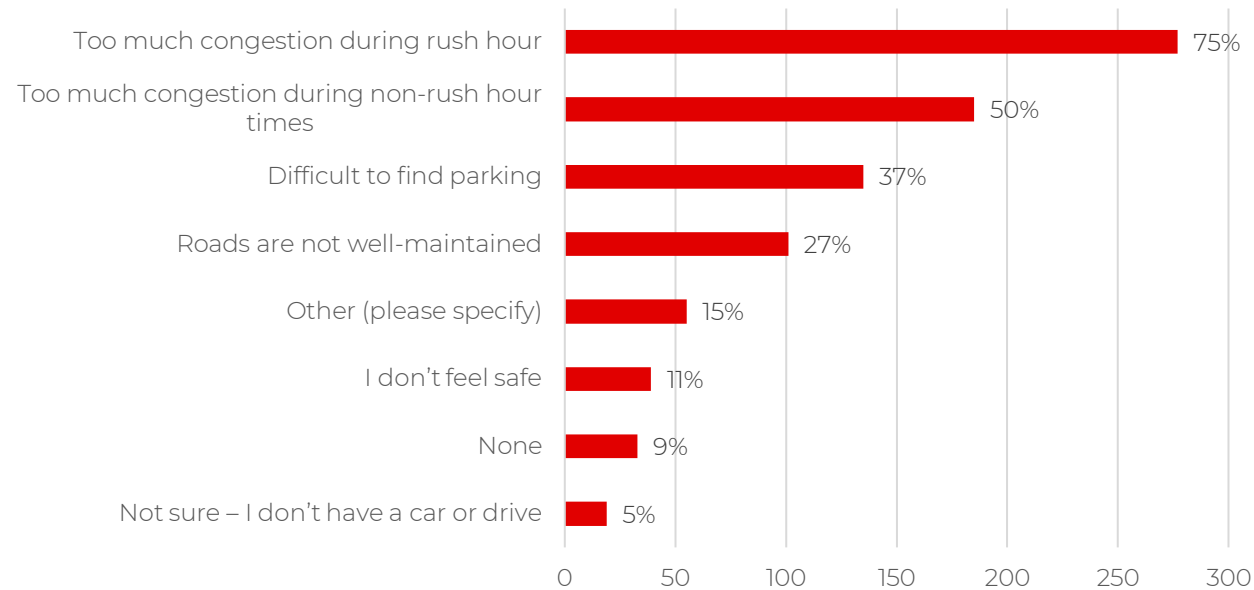
- Langley City's road network includes arterial, collector, and local roads, as well as an MoTI provincial highway
- Langley City operates over 50 traffic signals. MoTI and the Township of Langley also operate and maintain several traffic signals within the community



DRIVING

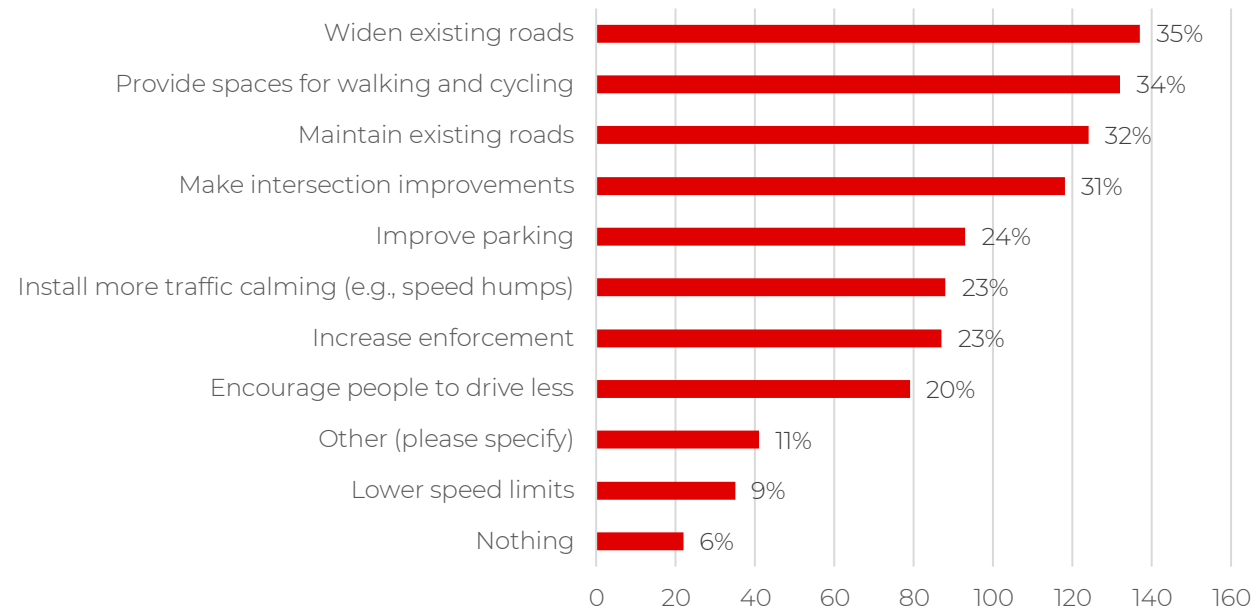
ISSUES AND OPPORTUNITIES (COMMUNITY SURVEY)

ISSUES



*n=368, 18 respondents identified they do not have a car or drive

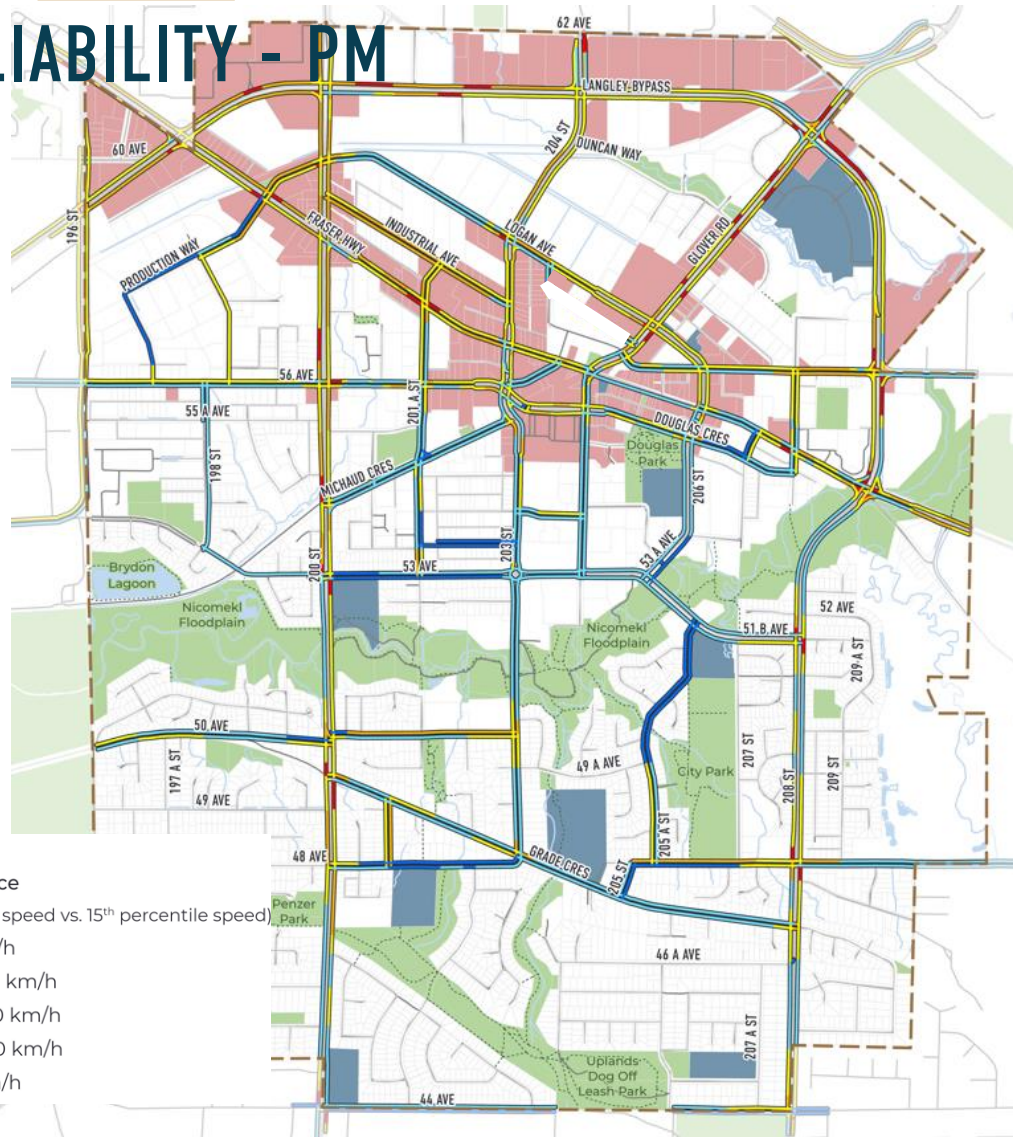
OPPORTUNITIES



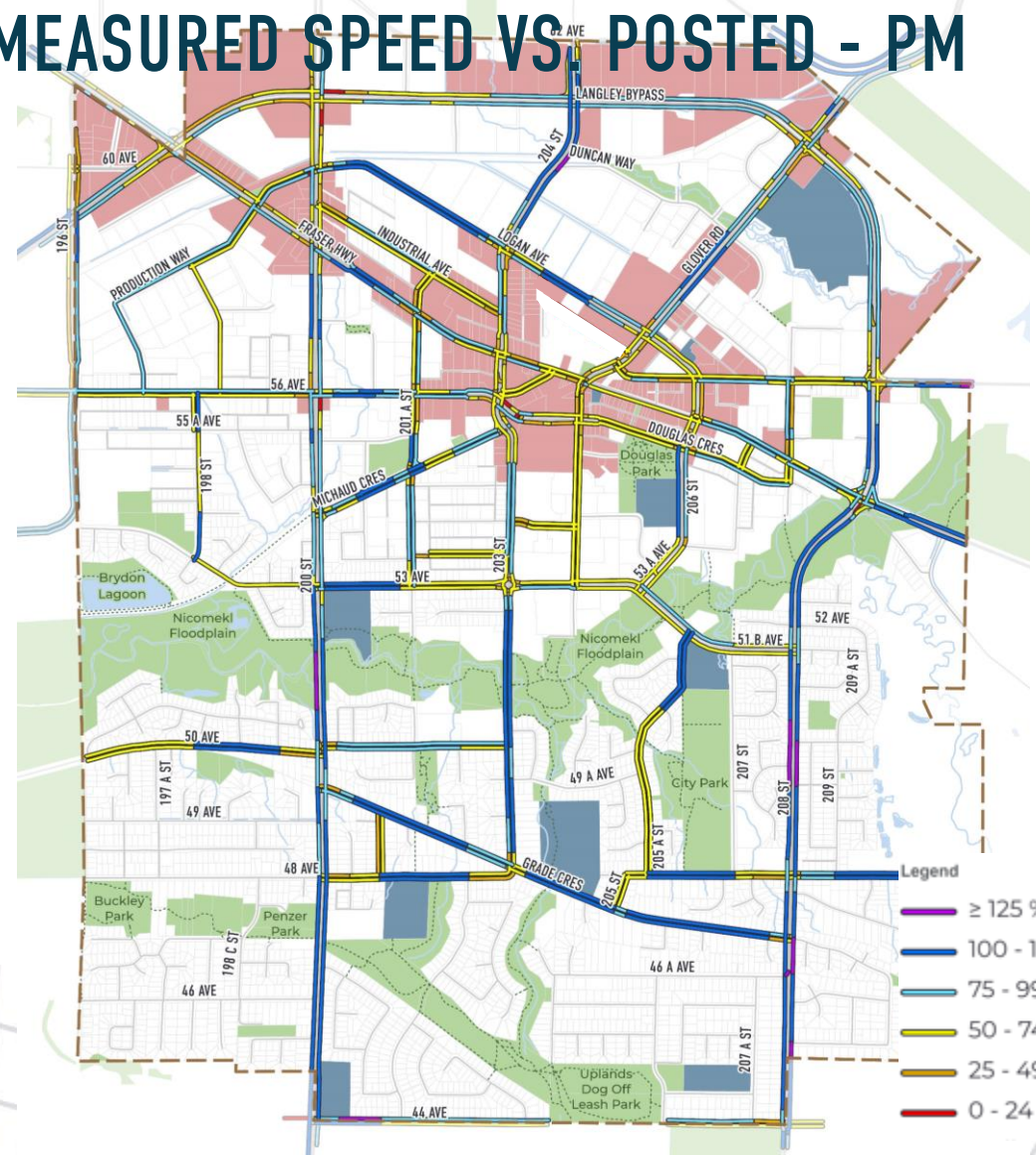
*n=386

TRAVEL SPEED / MOBILITY PERFORMANCE

RELIABILITY - PM



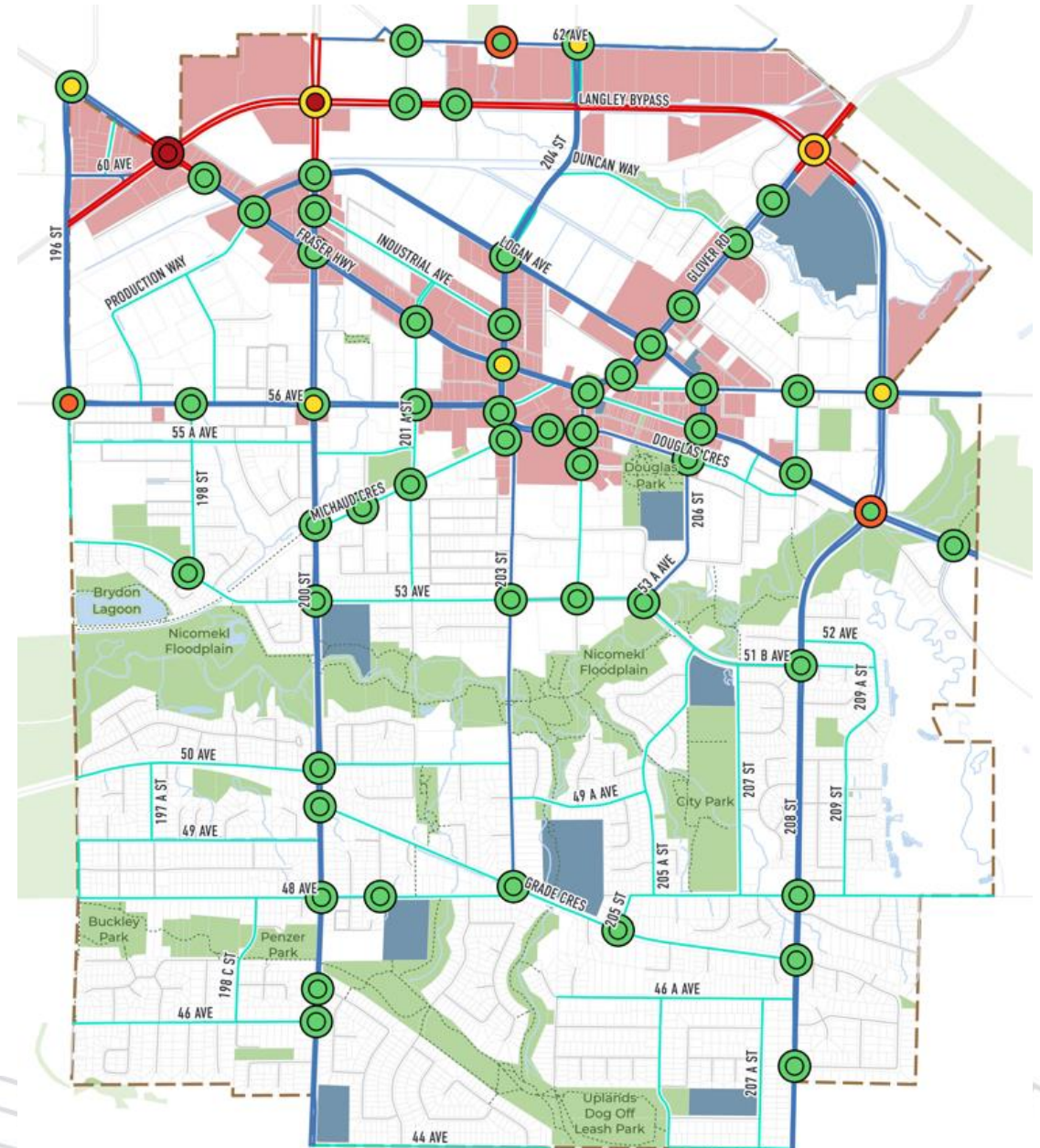
MEASURED SPEED VS. POSTED - PM



INTERSECTION PERFORMANCE

TODAY

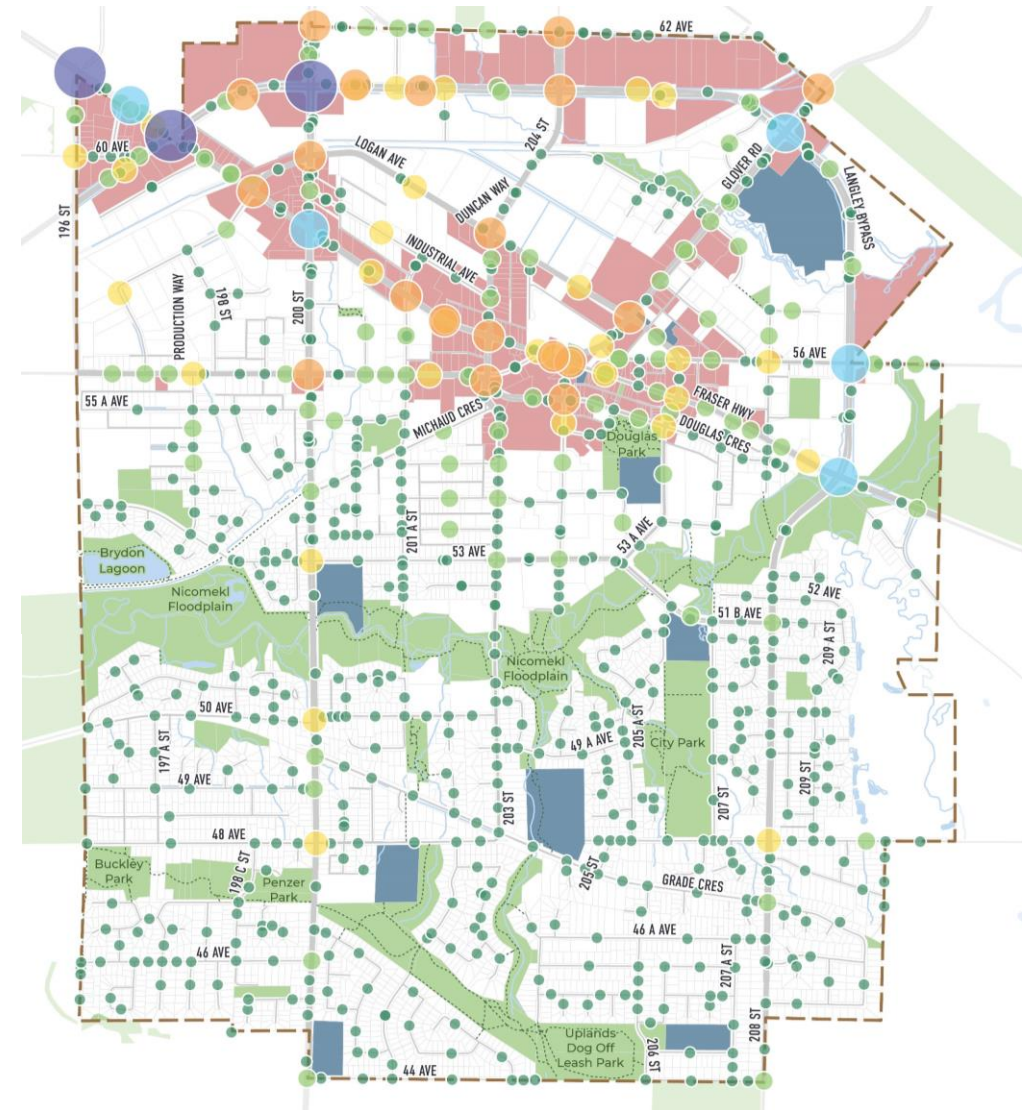
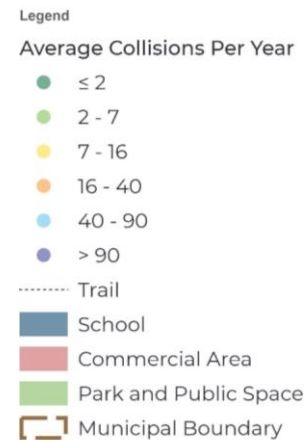
- Few intersection operational issues:
 - Langley Bypass
 - 208 Street



ROAD SAFETY

AVERAGE COLLISIONS PER YEAR

- Locations with the highest average number of collision per year
 - Langley Bypass (MoTI) & Fraser Highway
 - Fraser Highway & 196 Street (Township of Langley)
 - Langley Bypass (MoTI) & 200 Street

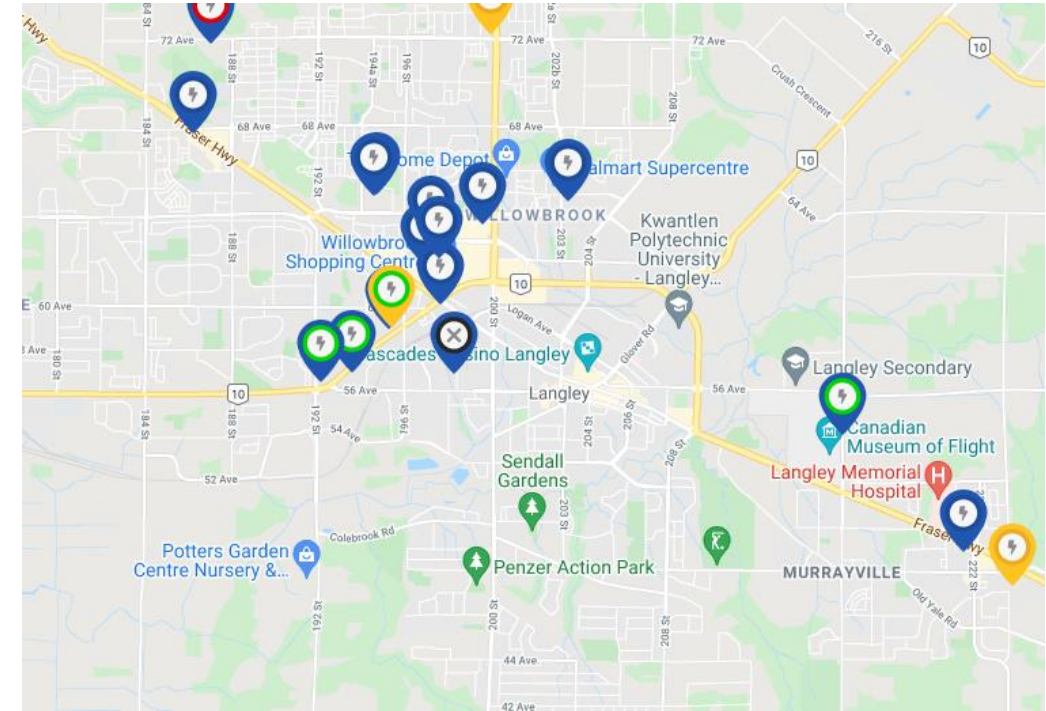


Source: ICBC Collision Data (2011-2020)

DRIVING

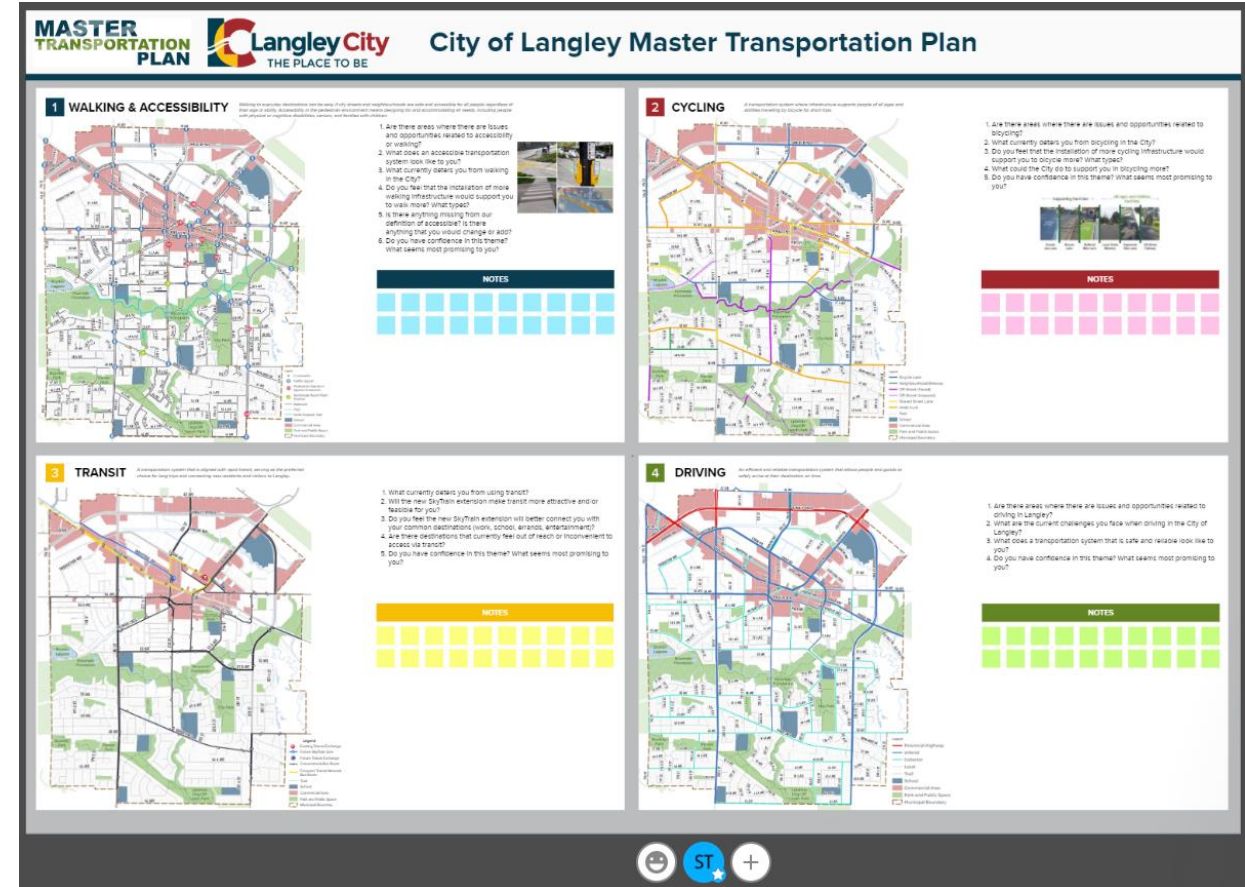
TRENDS AND OPPORTUNITIES

- Complete Streets
- Road Space Reallocation
- Goods Movement and Cyclologistics
- Electric Vehicles
- Car Share and Ride Hailing
- Autonomous Vehicles



QUESTIONS & DISCUSSION

- Look for the Mural link in the chat
- Enter your name
- Zoom in and out with scroll
- Type in comments in sticky notes
- Double click to create new sticky note



A light gray background featuring a complex, stylized map pattern of streets and blocks. A large, solid dark red rectangle is centered on the page, serving as a backdrop for the text.

CLOSING AND NEXT STEPS

QUESTIONS?

- Visit the project website: city.langley.bc.ca/cityhall/reports-plans/master-transportation-plan
- Feel free to email questions to: info@langleycity.ca

